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Anthony Lee Washington III
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HDR ENGINEERING, INC.
120 BRENTWOOD COMMONS WAY
SUITE 525
BRENTWOOD, TN 37027
ANTHONY L. WASHINGTON, III, P.E. 119749

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN1
TITLE SHEET	1
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS	1A
PROJECT COMMITMENTS	1B
ESTIMATED ROADWAY QUANTITIES	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2A
GENERAL NOTES.....	2B
SPECIAL NOTES.....	2C
UTILITY OWNERS.....	2C1
ENVIRONMENTAL NOTES.....	2D, 2D1
TABULATED QUANTITIES	2E

YEAR	PROJECT NO.	SHEET NO.
2025	NH-I-24-9(97)	ROADWAY-SIGN 1

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE
SHEET

Index Of Sheets
SEE SHEET NO. 1A

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

DAVIDSON COUNTY

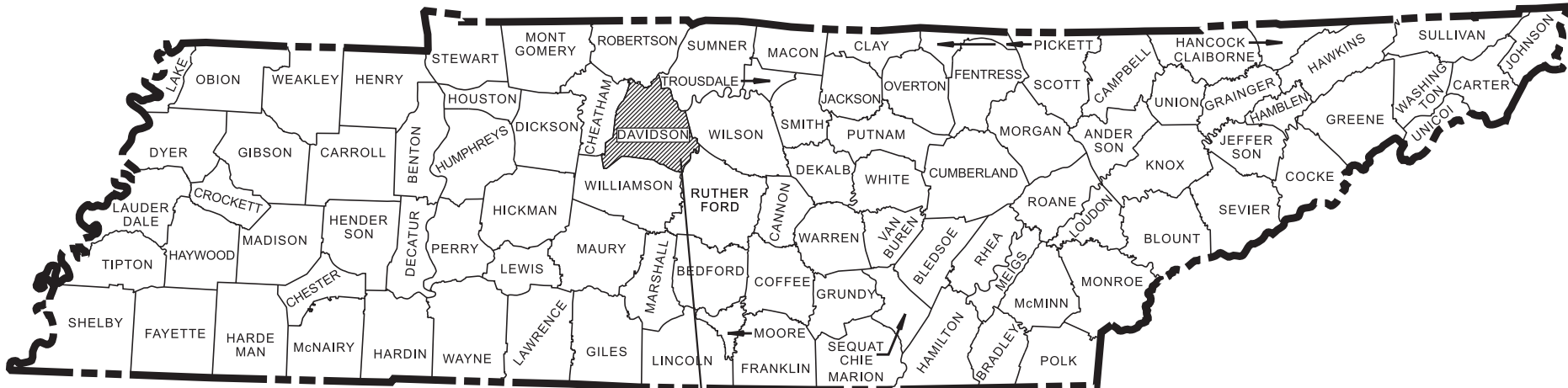
INTERSTATE 24
FROM: HAYWOOD LANE (L.M. 21.10)
TO: RUTHERFORD COUNTY LINE (L.M. 27.62)

RESURFACE
MILL & 411D

STATE HIGHWAY NO. N/A F.A.H.S. NO. I-24

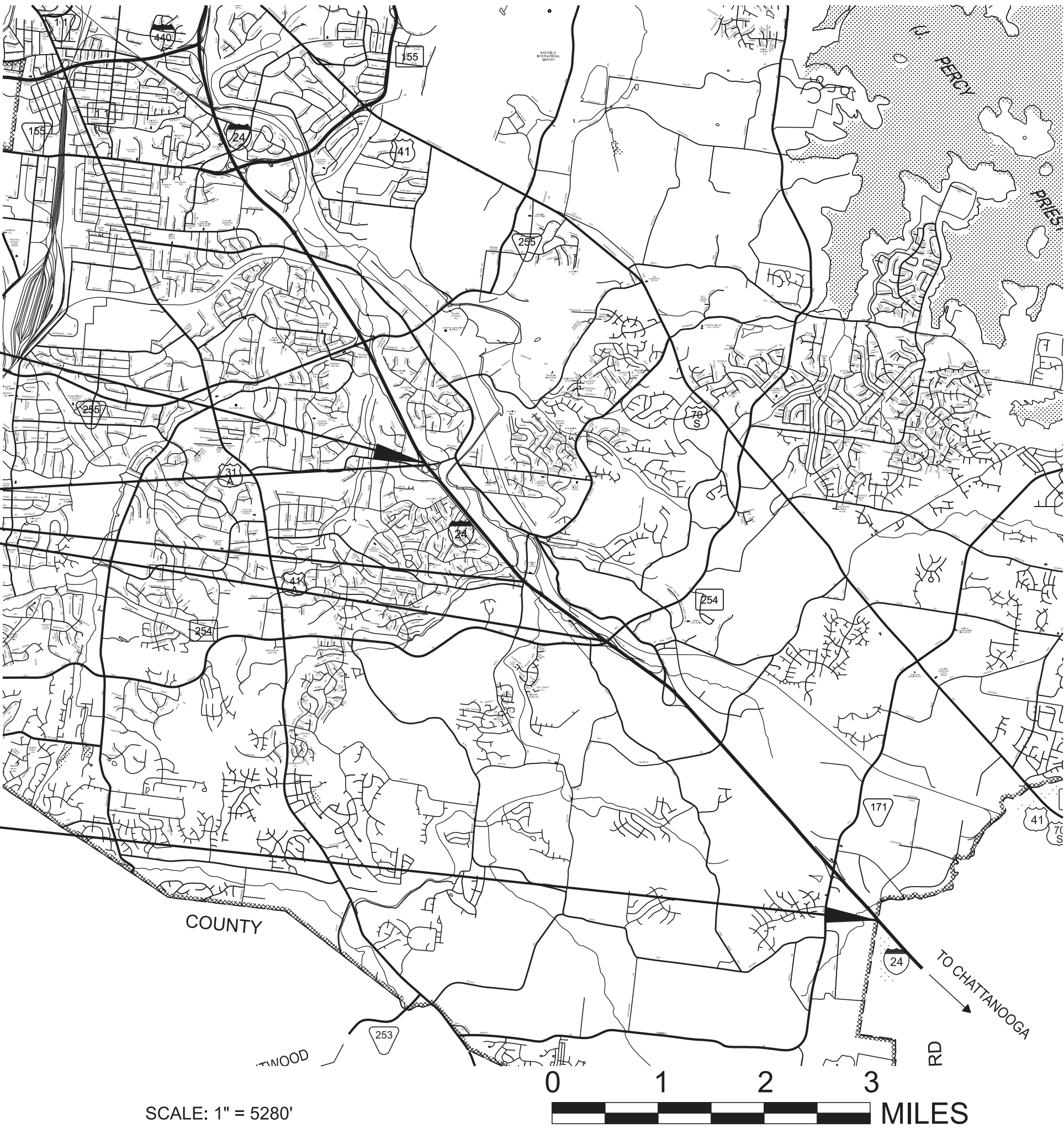
DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES X	NO

TENN.	YEAR	SHEET NO.
	2025	1
FED. AID PROJ. NO.	NH-I-24-9(97)	
STATE PROJ. NO.	19I024-F8-002	
STATE PROJ. NO.	19I024-M3-013	



PROJECT LOCATION
BRIDGE ID. # 19I00240033 19I00240035 19I00240037
19I00240039 19I00240311 19I00240041
19I00240313 19I00240043 19I00240085
19I00240045 19I00240047 19I00240049

EXCLUSIONS	
STATION TO STATION	LENGTH (FT.)
24.28 - 25.38	5808.0
-	.
-	.
TOTAL =	5808.0



NH-I-24-9(97)
BEGIN PROJECT NO. 19I024-F8-002 RESURFACE
L.M. 21.10

BRIDGE DECK REPAIR PROJECT NO. 19I024-M3-013
I-24@ L.M. 21.53, 23.17, & 23.92

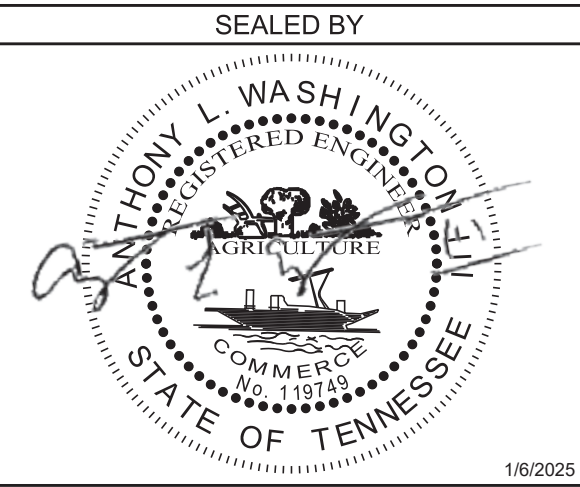
NH-I-24-9(97)
END PROJECT NO. 19I024-F8-002 RESURFACE
L.M. 27.62

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT PROJECT MANAGER: KONNER SPRADLIN, P.E.
DESIGNED BY : HDR ENGINEERING, INC.
DESIGNER : ANTHONY L. WASHINGTON, III, P.E. CHECKED BY DAVID HORNE, P.E.
P.E. NO. 98034-4194-04
PIN NO. 131211.00



APPROVED: WILL REID, CHIEF ENGINEER

DATE:

APPROVED: HOWARD H. ELEY, COMMISSIONER

TRAFFIC COUNTER & WEATHER STATIONS	
STATION LOCATION	LOG MILE
TC STATION 228	22.400
TC STATION 357	24.120
TC STATION 222	25.240

TRAFFIC DATA	
ADT (2025)	191,053
POSTED SPEED	70 MPH

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: _____
DIVISION ADMINISTRATOR DATE

ROADWAY INDEX

SHEET NAME

SHEET NO.

SIGNATURE SHEETS.....	ROADWAY-SIGN1
TITLE SHEET	1
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS.....	1A
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GENERAL NOTES.....	2B
SPECIAL NOTES.....	2C
UTILITY OWNERS.....	2C1
ENVIRONMENTAL NOTES.....	2D, 2D1
TABULATED QUANTITIES	2E
PAVEMENT MARKING IMPROVEMENTS.....	2F, 2F1 – 2F4
BRIDGE PLANS.....	B1

NOTE: THE ALPHABETICAL LETTERS “I”, “O” & “Q” ARE NOT USED IN THE NUMBERING OF SHEETS.

NO UTILITY SHEETS INCLUDED IN THIS SET OF PLANS.

STANDARD ROADWAY DRAWINGS

DWG.

REV.

DESCRIPTION

STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS

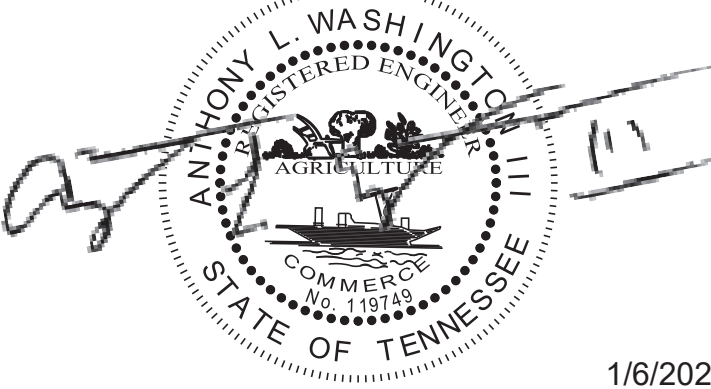
RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND

DESIGN - TRAFFIC CONTROL

T-M-5	03-01-23	MARKING DETAIL FOR FREEWAYS
T-M-6	03-01-23	MARKING DETAIL FOR EXPRESSWAY AND FREEWAY INTERCHANGES
T-M-7	06-28-19	GORE MARKING DETAILS FOR EXPRESSWAY & FREEWAY INTERCHANGES
T-M-8	06-28-19	MARKING DETAILS FOR EXPRESSWAYS & FREEWAYS
T-M-9	05-01-23	PAVEMENT MARKING AND SIGNING DETAILS FOR RAMP INTERSECTIONS
T-M-9A		PAVEMENT MARKING AND SIGNING DETAILS FOR RAMP INTERSECTIONS
T-M-9B		PAVEMENT MARKING AND SIGNING DETAILS FOR RAMP INTERSECTIONS
T-M-15	06-28-19	ASPHALT SHOULDER RUMBLE STRIP INSTALLATION DETAILS FOR INTERSTATE AND ACCESS CONTROLLED ROUTES
T-M-18A		DELINEATOR MOUNTING DETAILS
T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-11	03-04-21	ONE LANE CLOSURE DETAIL ON DIVIDED HIGHWAYS
T-WZ-12	03-04-21	ONE LANE CLOSURE DETAIL FOR BRIDGES ON DIVIDED HIGHWAYS
T-WZ-13	05-01-20	TWO-OUTSIDE LANE CLOSURE FOR EXPRESSWAY AND FREEWAYS
T-WZ-18	07-07-23	SHOULDER CLOSURE DETAIL FOR FREEWAYS AND DIVIDED HIGHWAYS
T-WZ-60		FREEWAY RESURFACING SIGNING LAYOUT
T-WZ-63	1-09-24	WORK ZONE IN THE VICINITY OF AN ENTRANCE RAMP
T-WZ-64		WORK ZONE IN THE VICINITY OF AN EXIT RAMP
T-WZ-FAB1		FLASHING YELLOW ARROW BOARD

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-24-9(97)	1A

SEALED BY



1/6/2025

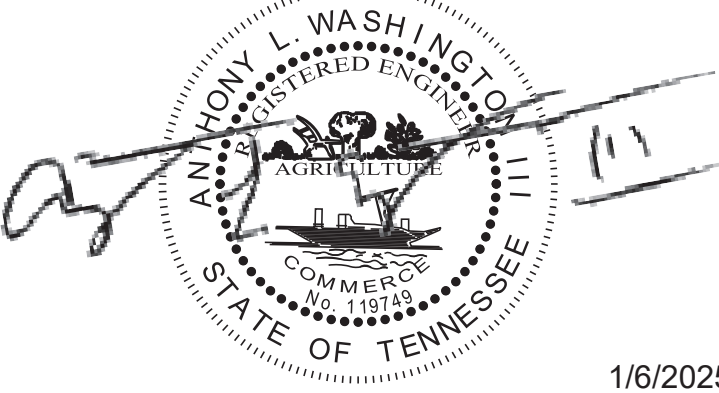
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ROADWAY INDEX
AND
STANDARD
ROADWAY
DRAWINGS

PROJECT COMMITMENTS			
COMMITMENT ID	SOURCE DIVISON	DESCRIPTION	STA. / LOCATION
EDHZ001	ENVIRONMENTAL DIVISION HAZARDOUS MATERIALS	ASBESTOS CONTAINING MATERIAL (ACM) SURVEYS WERE COMPLETED ON BRIDGE NO. 19I00240039 I-24 OVER MILL CREEK LM 23.17 (19-I0024-23.17) AND BRIDGE NO. 19I00240041 I-24 OVER BELL ROAD LM 23.92 (19-I0024-23.92). NO ACM WAS DETECTED. PLEASE SEE THE REPORTS FOR FURTHER DETAILS AND PHOTOGRAPHS. NO SPECIAL ACCOMMODATIONS FOR DEMOLITION AND WASTE DISPOSAL ARE ANTICIPATED FOR THESE STRUCTURES AND THE MATERIAL CAN BE DEPOSITED IN A C&D LANDFILL. PRIOR TO THE DEMOLITION OR REHABILITATION OF ANY STRUCTURE (BRIDGE OR BUILDING), THE CONTRACTOR IS REQUIRED TO SUBMIT THE NATIONAL EMISSION STANDARDS FOR HAZARDOUS AIR POLLUTANTS STANDARD 10-DAY NOTICE OF DEMOLITION TO THE TDEC DIVISION OF AIR POLLUTION CONTROL (PER TDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (JANUARY 1, 2021) SECTIONS 107.08.D AND 202.03).	BRIDGE NO. 19I00240039 I-24 OVER MILL CREEK LM 23.17 (19-I0024-23.17) BRIDGE NO. 19I00240041 I-24 OVER BELL ROAD LM 23.92 (19-I0024-23.92)
EDHZ002	ENVIRONMENTAL DIVISION HAZARDOUS MATERIALS	AN ASBESTOS CONTAINING MATERIAL (ACM) SURVEY WAS COMPLETED ON BRIDGE NO. 19I00240033 I-24 OVER HAYWOOD LANE LM 21.53 (19-I0024-21.53). NO ASBESTOS WAS DETECTED. PLEASE SEE THE REPORT FOR FURTHER DETAILS AND PHOTOGRAPHS. NO SPECIAL ACCOMMODATIONS FOR DEMOLITION AND WASTE DISPOSAL ARE ANTICIPATED FOR THESE STRUCTURES AND THE MATERIAL CAN BE DEPOSITED IN A C&D LANDFILL. PRIOR TO THE DEMOLITION OR REHABILITATION OF ANY STRUCTURE (BRIDGE OR BUILDING), THE CONTRACTOR IS REQUIRED TO SUBMIT THE NATIONAL EMISSION STANDARDS FOR HAZARDOUS AIR POLLUTANTS STANDARD 10-DAY NOTICE OF DEMOLITION TO THE TDEC DIVISION OF AIR POLLUTION CONTROL (PER TDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (JANUARY 1, 2021) SECTIONS 107.08.D AND 202.03).	BRIDGE NO. 19I00240033 I-24 OVER HAYWOOD LANE LM 21.53 (19-I0024-21.53)

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-24-9(97)	1B

SEALED BY



1/6/2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PROJECT
COMMITMENTS

ESTIMATED ROADWAY QUANTITIES				
	ITEM NO.	DESCRIPTION	UNIT	QUANTITY 19I024-F8-002
(1)(2)	208-01.05	BROOMING & DEGRASSING SHOULDERS	L.M.	22
(1)	303-01	MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON	1435
	403-01	BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	201
(3)	411-01.21	LONGITUDINAL JOINT SEALANT	L.M.	60
(4)	411-03.10	ACS MIX(PG76-22) GRADING D	TON	32036
	411-12.01	SCORING SHOULDERS (CONTINUOUS) (16IN WIDTH)	L.M.	21.7
(5)	415-01.01	COLD PLANING BITUMINOUS PAVEMENT	TON	31734
(6)	604-04.02	APPLIED TEXTURE FINISH (EXISTING STRUCTURES)	S.Y.	35768
(7)	712-01	TRAFFIC CONTROL	LS	1
	712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	400
	712-05.03	WARNING LIGHTS (TYPE C)	EACH	100
(8)	712-06	SIGNS (CONSTRUCTION)	S.F.	2984
(9)	712-06.16	SIGNS (CONSTRUCTION)(REDUCED SPEED WARNING)	EACH	11
	712-07.03	TEMPORARY BARRICADES (TYPE III)	L.F.	24
	712-08.03	ARROW BOARD (TYPE C)	EACH	2
	712-08.08	SPEED FEEDBACK SIGN ASSEMBLY	EACH	2
	712-08.09	DIGITAL SPEED LIMIT SIGN ASSEMBLY	EACH	10
	712-08.12	QUEUE PROTECTION TRUCK	DAY	120
	713-16.01	CHANGEABLE MESSAGE SIGN UNIT	EACH	2
	713-16.20	SIGNS (R5-1A)	EACH	4
	716-01.22	SNOWPLOWABLE RAISED PAVMENT MARKERS (MONO-DIR)(1 COLOR)	EACH	28
	716-01.23	SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR)(2 COLOR)	EACH	3527
	716-01.30	REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH	3469
(10)(11)	716-02.05	PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	54
(10)(11)	716-02.06	PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	2
(10)(11)	716-02.07	PLASTIC PAVEMENT MARKING (24" BARRIER LINE)	L.F.	1950
(10)(11)	716-04.01	PLASTIC PAVEMENT MARKING (STRAIGHT-TURN ARROW)	EACH	2
(10)(11)	716-04.02	PLASTIC PAVEMENT MARKING(DOUBLE TURNING ARROW)	EACH	2
(10)(11)	716-04.05	PLASTIC PAVEMENT MARKING (STRAIGHT ARROW)	EACH	4
(10)(11)	716-04.06	PLASTIC PAVEMENT MARKING (WRONG WAY ARROW)	EACH	2
(10)(11)	716-04.07	PLASTIC PAVEMENT MARKING (EXIT ONLY ARROW)	EACH	11
(10)(11)	716-04.09	PLASTIC PAVEMENT MARKING (H.O.V. DIAMOND)	EACH	43
(10)(11)	716-04.12	PLASTIC PAVEMENT MARKING (YIELD LINE)	S.F.	27
(10)(11)	716-04.14	PLASTIC PAVEMENT MARKING (LANE REDUCTION ARROW)	EACH	3
(12)	716-05.20	PAINTED PAVEMENT MARKING (6" LINE)	L.M.	26
(10)	716-12.02	ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.	26
(10)	716-12.03	ENHANCED FLATLINE THERMO PVMT MRKNG (8IN BARRIER LINE)	L.F.	4190
(10)	716-12.05	ENHANCED FLATLINE THERMO PVMT MRKNG (6IN DOTTED LINE)	L.F.	1614
(10)	716-12.08	ENHANCED FLAT LINE THERMO (12IN BARRIER LINE)	L.F.	2710
(10)	716-12.10	ENHANCED FLAT LINE THERMO (12IN DOTTED)	L.F.	29682
(13)	716-15.10	PREFORMED PERMANENT TAPE (6IN LINE)	L.F.	3694
(13)	716-15.12	PREFORMED PERMANENT TAPE (12IN LINE)	L.F.	240
	717-01	MOBILIZATION	LS	1

FOOTNOTES	
(1)	TO BE USED AS DIRECTED BY THE ENGINEER.
(2)	NO DIRT OR DEBRIS TO BE LEFT ON SHOULDER INCLUDING UNDER AND AROUND GUARDRAIL THIS WORK TO BE PERFORMED BEFORE ALL OTHER OPERATIONS. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING COLD PLANNING AND ASPHALT DEBRIS DOES NOT ENTER THE DRAINS AND WILL BE RESPONSIBLE FOR CLEAN OUTS.
(3)	TO BE USED FOR SEALING OF ALL SURFACE LAYER CONSTRUCTION JOINTS ALONG THE TRAVEL LANES AND SHOULDERS AS DIRECTED BY THE ENGINEER. USE CRAFTCO PAVEMENT JOINT ADHESIVE #34524. PAVON JOINT ADHESIVE BY PAVON CORPORATION OR DENSO TAPE BY DENSO. INCLUDES 1382 TONS FOR RAMPS UP UNTIL THE CONCRETE STARTS AND GORE AREAS ALONG MAINLINE.
(4)	INCLUDES 1369 TONS FOR RAMPS UP UNTIL THE CONCRETE STARTS AND GORE AREAS ALONG MAINLINE.
(6)	SEE "APPLIED TEXTURE FINISH" TABLE ON SHEET 2E FOR LOCATIONS. THE COLOR IS TO BE "WHITE, FEDERAL SPECIFICATION NO. 37886." COST TO INCLUDE REPLACING BARRIER WALL DELINEATORS. ALL VEGETATION MUST BE REMOVED FROM THE BARRIER WALL. THIS WORK WILL BE INCLUDED AS PART OF THIS ITEM NUMBER.
(7)	THE CONTRACTOR SHALL COMPLY WITH SECTION 712 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION REGARDING TEMPORARY TRAFFIC CONTROL AND THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
(8)	THE CONTRACTOR IS RESPONSIBLE FOR THE STAKING OF CONSTRUCTION SIGNS. IN THE EVENT THAT A CONSTRUCTION AND/OR REGULATORY SIGN IS TEMPORARILY DESIGNATED NOT IN USE DURING THE CONSTRUCTION PHASE OF A PROJECT, THE CONTRACTOR SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 712-06 SIGNS (CONSTRUCTION)
(9)	ITEM TO BE USED ONLY WHEN A REDUCED SPEED LIMIT IS ESTABLISHED WITHIN THE PROJECT CONSTRUCTION WORK ZONE LIMITS. ITEM INCLUDES SIGN FACE, SUPPORTS, AND TWO TYPE “B” FLASHERS PER THE STANDARD SPECIFICATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TURNING ON THE TYPE “B” FLASHERS WHEN WORKERS ARE IN THE CONSTRUCTION WORK ZONE AND TURNING THEM OFF WHEN WORKERS ARE NO LONGER IN THE CONSTRUCTION WORK ZONE.
(10)	ITEM TO BE USED AS PERMANENT MARKING ONLY.
(11)	THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
(12)	ITEM TO BE USED AS TEMPORARY PAVEMENT MARKING ONLY.
(13)	TO BE USED FOR CONCRETE SURFACES. INCLUDES 2471 LF OF SOLID YELLOW / WHITE LINE FOR EDGELINES, AND 1223 LF OF CONTRAST SHADOWED MARKING FOR LANES LINES.

THERE ARE NO UTILITY ADJUSTMENTS ON THIS PROJECT

THERE IS NO GUARDRAIL WORK ON THIS PROJECT

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-24-9(97)	2

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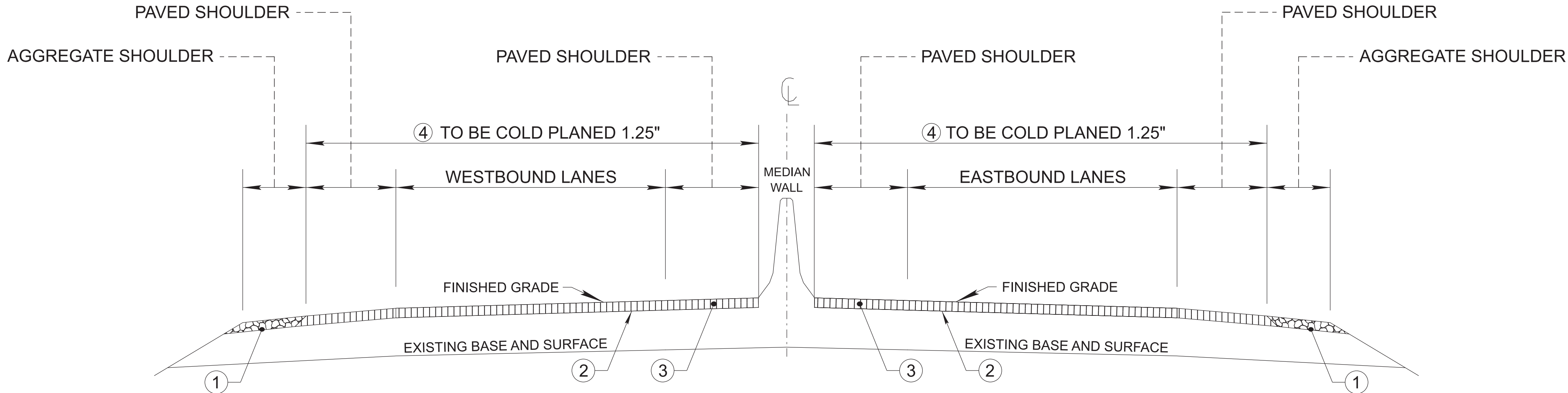
1/6/2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED
ROADWAY
QUANTITIES

SEGMENT	BEGINNING L.M.	ENDING L.M.	LENGTH (MILES)	LANE WIDTH (FEET)	# OF LANES (WESTBOUND)	SHOULDER WIDTH (WESTBOUND)	# OF LANES (EASTBOUND)	SHOULDER WIDTH (EASTBOUND)
1	21.10	21.31	0.21	12	5	19	5	19
2	21.31	21.45	0.14	12	5	18	5	22
3	21.45	21.57	0.12	12	5	18	5	18
4	21.57	21.75	0.18	12	4	20	4	20
5	21.75	21.84	0.09	12	4	20	5	22
6	21.84	21.94	0.1	12	4	20	4	22
7	21.94	23.60	1.66	12	4	19	5	19
8	23.60	23.68	0.08	12	5	20	5	22
9	23.68	23.81	0.13	12	5	20	5	20
10	23.81	24.06	0.25	12	4	26	4	28
11	24.06	24.19	0.13	12	5	20	4	26
12	24.19	24.28	0.09	12	5	10	5	10
13*	24.28	24.42	0.14	12	5	10	5	10
14*	24.42	24.49	0.07	12	4	21	5	20
15*	24.49	24.54	0.05	12	4	20	5	20
16*	24.54	24.71	0.17	12	4	20	4	20
17*	24.71	24.72	0.01	12	4	20	5	20
18*	24.72	24.91	0.19	12	5	20	4	21
19*	24.91	24.98	0.07	12	4	20	4	21
20*	24.98	25.20	0.22	12	4	20	5	21
21*	25.20	25.38	0.18	12	4	20	4	21
22	25.38	25.84	0.46	12	4	20	4	21
23	25.84	26.06	0.22	12	4	26	4	14
24	26.06	26.18	0.12	12	5	26	4	14
25	26.18	26.41	0.23	12	5	14	4	26
26	26.41	26.53	0.12	12	5	14	5	28
27	26.53	26.71	0.18	12	5	14	4	26
28	26.71	27.29	0.58	12	4	20	4	20
29	27.29	27.44	0.15	12	4	20	5	20
30	27.44	27.62	0.18	12	4	26	4	14

* EXCLUSIONS



TYPICAL SECTION
I-24

PROPOSED PAVEMENT SCHEDULE	
①	MINERAL AGGREGATE @ 2" THICK ITEM NO. 303-01 MINERAL AGGREGATE, TYPE "A" BASE, GRADING "D"
②	TACK COAT (TC) ITEM NO. 403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC) SEE D.G. 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD.
③	ASPHALTIC CONCRETE SURFACE (HM) 1¼" TH. (APPROX. 132.5 LBS/SY) ITEM NO. 411-03.10 ACS MIX (PG76-22) GRADING D
④	COLD PLANING 1¼" TH. (APPROX. 131.25 LBS/SY) ITEM NO. 415-01.01 COLD PLANING BITUMINOUS PAVEMENT

VERTICAL CLEARANCES			
THE CONTRACTOR SHALL: MAINTAIN THE FOLLOWING MINIMUM VERTICAL CLEARANCES.			
LOCATION	BRIDGE NO.	EBL	WBL
L.M. 22.98	19I00240037	17.67'	16.08'
L.M. 24.61	19I00240085	17.42'	17.08'
L.M. 25.39	19I00240045	16.92'	16.92'
L.M. 26.97	19I00240047	17.50'	16.83'

BRIDGE RECOMMENDATIONS			
LOCATION	BRIDGE NO.	LENGTH (FT.)	TREATMENT
L.M. 21.53	19I00240033	153.50	SEE BRIDGE REPAIR PLANS
L.M. 22.80	19I00240035	32.00	PAVE WITH PLANS MIX/TREATMENT TYPE
L.M. 23.17	19I00240039	167.00	SEE BRIDGE REPAIR PLANS
L.M. 23.90	19I00240311	54.00	PAVE WITH PLANS MIX/TREATMENT TYPE
L.M. 23.92	19I00240041	118.53	SEE BRIDGE REPAIR PLANS
L.M. 23.94	19I00240313	47.00	LEAVE AS IS
L.M. 24.21	19I00240043	31.00	PAVE WITH PLANS MIX/TREATMENT TYPE
L.M. 27.47	19I00240049	23.00	PAVE WITH PLANS MIX/TREATMENT TYPE

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-24-9(97)	2A

SEALED BY

1/6/2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL
SECTIONS AND
PAVEMENT
SCHEDULE

GENERAL NOTES

GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

MISCELLANEOUS

- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

- (2) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20, PAINTED PAVEMENT MARKING (6" LINE), L.M.

FINAL PAVEMENT MARKING

- (6) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:
 - a. SHOULDERS SHALL BE BROOMED AND DE-GRASSED AND MATERIAL SHALL BE PICKED UP AND REMOVED. THIS WILL BE PAID FOR UNDER ITEM NO. 208-01.05.
 - b. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (9) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.
- (10) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 8" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.03, ENHANCED FLATLINE THERMO PVMT MRKNG (8IN BARRIER LINE), L.F. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

SNOWPLOWABLE REFLECTIVE PAVEMENT MARKERS

- (20) REMOVE EXISTING SNOWPLOWABLE MARKERS PRIOR TO PAVING AND/OR COLD PLANING. REMOVE ALL ADHESIVES PRIOR TO PAVING. PATCH ANY HOLES OR DIVOTS RESULTING FROM THE REMOVAL OF A MARKER IN A MANNER WHICH ENSURES A UNIFORM PAVED SURFACE. PATCH WORK SHALL BE INCLUDED WITH COST OF OTHER ITEMS OF CONSTRUCTION.

PAVEMENT

PAVING

- (2) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

- (9) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

SIGNING

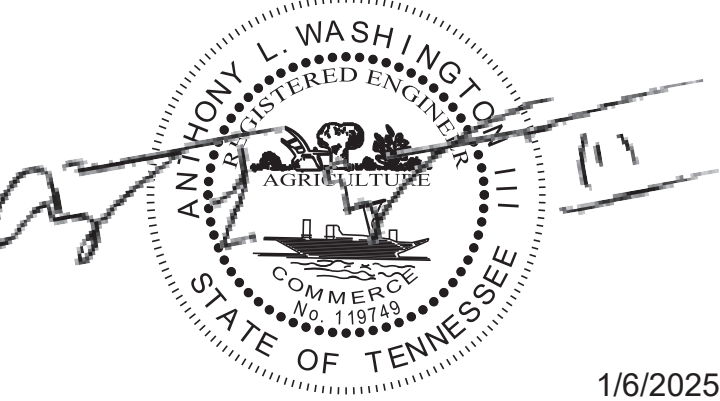
- (12) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GENERAL
NOTES

SPECIAL NOTES

MISCELLANEOUS

- (1)

THE CONTRACTOR SHALL TAKE EXTREME CARE WHEN WORKING AROUND EXISTING GUARDRAIL, HIGHWAY SIGNS, OR ANY DEPARTMENT OF TRANSPORTATION STRUCTURE TO AVOID DAMAGE DURING THE COURSE OF CONSTRUCTION. ANY DAMAGE WILL BE REPLACED OR REPAIRED AT HIS OWN EXPENSE.
- (2)

ALL SIGNING AND PAVEMENT MARKINGS ARE TO BE IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

PAVEMENT MARKINGS

- (1)

THE CONTRACTOR IS TO PROVIDE THE LAYOUT OF ALL EXISTING PAVEMENT MARKINGS BEFORE BEGINNING THE RESURFACING PROJECT, EXISTING MARKINGS SHALL BE REFERENCED AS NEEDED TO ENSURE THAT ALL NEW MARKINGS ARE REPLACED IN THEIR CORRESPONDING LOCATIONS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

PAVEMENT

RESURFACING

- (1)

SURFACE IS TO BE CROWNED AS DIRECTED BY THE TDOT OPERATIONS DISTRICT ENGINEER.
- (2)

THE CONTRACTOR WILL PLACE THE SPOT LEVELING WHERE AND AS DIRECTED BY THE TDOT OPERATIONS DISTRICT ENGINEER.
- (3)

COLD PLANING IS TO BE DONE IMMEDIATELY PRIOR TO THE COMMENCEMENT OF PAVING OPERATIONS. COLD PLANED SURFACES ARE NOT TO BE LEFT EXPOSED FOR MORE THAN FOUR (4) CALENDAR DAYS (96 HOURS) BEFORE BEING OVERLAYED BY THE FIRST COURSE OF ASPHALT. IF NECESSARY, COLD PLANING OPERATIONS ARE TO BE SUSPENDED TO ALLOW PAVING OPERATIONS TO CATCH UP. AFTER COLD PLANING AND PRIOR TO ALLOWING USE BY TRAFFIC, THE CONTRACTOR WILL RAMP ASPHALT AROUND EXISTING UTILITIES TO ELIMINATE HAZARD TO VEHICLES. ALL COLD PLANING REQUIRED ON CITY STREETS OR SIDE
- (4)

TEMPORARY RAMP CLOSURES FOR MAILINE RESURFACING SHALL BE AS FOLLOW:

a.

RAMP CLOSURES SHOULD BE LIMITED TO NO MORE THAN 3 HOURS.

b.

ALL RAMP CLOSURE REQUESTS SHALL BE MADE AT LEAST 7 WORKING DAYS PRIOR TO THE ANTICIPATED DATE OF CLOSURE.

c.

RAMPS IN THE OPPOSITE DIRECTION OF THE CLOSURES CANNOT BE CLOSED AT SAME TIME AND NO SUBSEQUENT EXIT RAMPS SHALL BE CLOSED UNLESS IT IS LESS THAN 1 MILE FROM RAMP TO RAMP.

d.

RAMPS WITH HOSPITALS OR OTHER EMERGENCY/FIRE/POLICE FACILITIES SHALL BE PASSABLE FOR EMERGENCY VEHICLES WHEN CLOSED TO REGULAR VEHICLE TRAFFIC.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1)

THE CONSTRUCTION SIGNS, FLAGMEN AND OTHER TRAFFIC CONTROL DEVICES WILL BE IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (2)

THE CONTRACTOR SHALL KEEP ALL TRAFFIC LANES OPENED TO TRAFFIC DURING NON-WORK HOURS AND NON-WORK DAYS.
- (3)

THE DEPARTMENT SHALL RESERVE THE RIGHT TO REOPEN LANES AS NECESSARY WHEN TRAFFIC CONDITIONS ARE DEEMED UNACCEPTABLE (EXCESSIVE QUEUE LENGTHS AND DELAY TIMES). THE CONTRACTOR SHALL BE REQUIRED TO FULLY COOPERATE WITH THE PROJECT SUPERVISOR WHEN REQUESTED TO MAKE CHANGES TO THE TRAFFIC CONTROL.

- (4)

MESSAGE BOARDS SHALL BE USED NEAR INTERCHANGE AND/OR OTHER DESIGNATED AREAS IN ADVANCE OF THE WORK ZONE TO ALERT MOTORISTS OF POSSIBLE DELAYS AND RECOMMEND THE USE OF ALTERNATE ROUTES. THE MESSAGE SHALL BE UPDATED AS OFTEN AS NECESSARY SO THAT THE MOTORISTS OBTAIN CURRENT TRAFFIC INFORMATION. MESSAGE BOARDS SHALL BE RELOCATED AS NECESSARY AS THE WORK PROGRESSES. THE CONTRACTOR SHALL BE REQUIRED TO IDENTIFY AN INDIVIDUAL WORKING ON THE PROJECT THAT WILL BE RESPONSIBLE FOR KEEPING THE MESSAGES CURRENT AND RELOCATING MESSAGE BOARDS AS REQUESTED BY TDOT. THE PROJECT SUPERVISOR SHALL HAVE THE AUTHORITY TO APPROVE ALL MESSAGES AND REQUIRED CHANGES AT ANY TIME DUE TO CHANGING TRAFFIC CONDITIONS.

JOINT SEALANTS

- (1)

THE CONTACT SURFACE OF TRANSVERSE JOINTS AND LONGITUDINAL JOINTS IN THE SURFACE LIFT SHALL BE SEALED BY APPLYING JOINT SEALANT PRIOR TO PLACEMENT OF ADDITIONAL ASPHALT AGAINST THE PREVIOUSLY PLACED MATERIAL. MANUFACTURER’S RECOMMENDATIONS SHALL BE FOLLOWED IF THE MATERIAL NEEDS TO BE RE-HEATED, AND WHEN PLACING THE THIN, UNIFORM COAT.
- (2)

PRIOR TO APPLICATION OF THE SEALANT, THE FACE OF THE JOINT SHALL BE THOROUGHLY DRY AND FREE FROM DUST OR ANY OTHER MATERIAL THAT WOULD PREVENT PROPER SEALING. ALL JOINTS SHALL BE SWEEPED OR BLOWN FREE OF LOOSE MATERIAL, DIRT, VEGETATION, AND OTHER DEBRIS BY MEANS OF COMPRESSED AIR OR A POWER SWEEPER.
- (3)

TRUCK AND VEHICLE TRAFFIC SHALL NOT DRIVE ACROSS A SEALED JOINT UNTIL IT HAS DRIED SUFFICIENT TO PREVENT DAMAGE FROM TRACKING.

WORK RESTRICTIONS

- (1)

ALL CONSTRUCTION ACTIVITIES WILL BE DONE AT NIGHT AS DIRECTED BY THE ENGINEER. NO DAYTIME LANE CLOSURES WILL BE PERMITTED. NO LANE CLOSURES DURING SPECIAL EVENTS IN NASHVILLE.

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

- (1)

DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:

a.

WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

b.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

c.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

d.

WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
- UTILITY
- (2)

UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3)


THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

(4)

PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED ‘AROUND’ UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR’S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.

(5)

THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.
- | | | | |
|--------|------|---------------|-----------|
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| RESURF | 2025 | NH-I-24-9(97) | 2C |
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- SEALED BY



1/6/2025
- STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SPECIAL
NOTES
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UTILITY OWNERS

TELEPHONE/FIBER:

AT&T

116 SOUTH CANNON AVENUE
MURFREESBORO, TN 37129
CONTACT: LEE KORNEGAY
OFFICE PHONE: 615 848 2082
CELL PHONE:
Email: KK4096@ATT.COM

ELECTRIC:

MIDDLE TN ELECTRIC MEMBERSHIP

555 NEW SALEM ROAD
MURFREESBORO, TN 37129
CONTACT: CHRISTOPHER WEAVER
OFFICE PHONE: 615 494 1068
Email: CHRISWEAVER@MTE.COM

WATER AND SEWER:

METRO WATER & SEWER

1600 SECOND AVENUE NORTH
NASHVILLE, TN 37208
CONTACT: GEORGE REAGAN
OFFICE PHONE: 615 862 4572
Email: GEORGE.REAGAN@HASHVILLE.GOV

GAS:

TOWN OF SMYRNA

315 LOWERY ST.
SMYRNA, TN 37167
CONTACT: MIKE STRANGE
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FIBER OPTIC:

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520 WHALEY ST.
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Email: JOHN.BOEDEKER@LUMEN.COM

FIBER OPTIC:

XO COMMUNICATIONS (VERIZON)

101 MOLLOY ST., SUITE 300
NASHVILLE, TN 37201
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Email: ERIC.HAGA@VERIZON.COM

FIBER OPTIC:

WINDSTREAM COMMUNICATIONS

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COMCAST (XFINITY)

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FIBER OPTIC:

ZAYO BANDWITH

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FIBER OPTIC:

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ELECTRIC:

NASHVILLE ELECTRIC SERVICE

1214 CHURCH STREET, ROOM 363
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WATER AND SEWER:

CITY OF LAVERGNE-WATER/SEWER

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GAS:

PIEDMONT GAS (FORMERLY NASHVILLE GAS)

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GAS:

TC ENERGY (COLUMBIA GULF TRANSMISSION (HARTSVILLE))

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-24-9(97)	2C1

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1/6/2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

UTILITY OWNERS

ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (4) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

- (11) SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12) IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ENGINEERING PRODUCTION SUPPORT DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

- (1) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

ECOLOGY

- (2) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (3) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.
- (4) ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

PROJECT COMMITMENTS

- (5) SEE PROJECT COMMITMENTS, SHEET 1B, FOR DETAILS RELATING TO SPECIAL ENVIRONMENTAL COMMITMENTS REQUIRED BY THIS PROJECT.

SCOPE OF WORK

- (6) THIS PROJECT INCLUDES COLD PLANING, PLACING NEW ASPHALT SURFACE, PAVEMENT MARKINGS, TRAFFIC CONTROL AND BROOMING AND DEGRASSING.

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

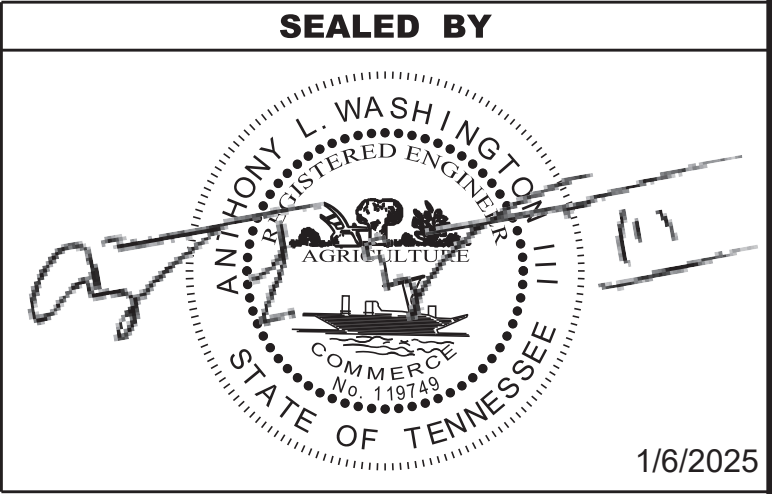
SEDIMENT CONTROL

- (6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (35) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-24-9(97)	2D



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL
NOTES


ENVIRONMENTAL NOTES (CONTINUOUS)

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-24-9(97)	2D1

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1/6/2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

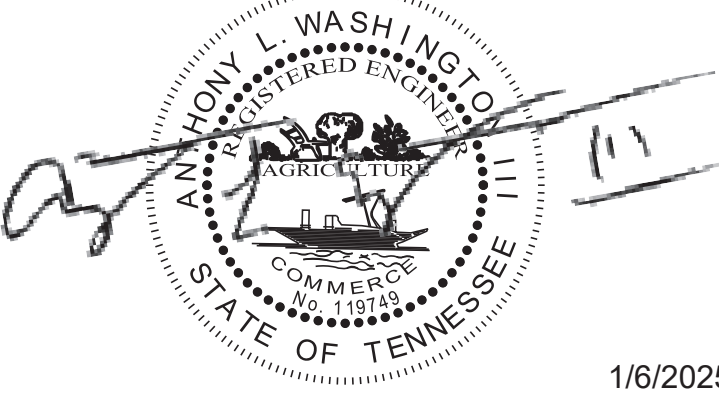
ENVIRONMENTAL
NOTES

TRAFFIC CONTROL SIGN TABULATION (RESURFACING)					
M.U.T.C.D. SIGN NO.	LEGEND \ DESCRIPTION	SIZE			ITEM NO. 712-06 S.F.
		L	x	W	
E5-1	EXIT	72"	x	60"	30
E5-2	EXIT OPEN	48"	x	36"	12
E5-2a	EXIT CLOSED	48"	x	36"	12
G20-1	ROAD WORK NEXT 7 MILES	64"	x	24"	11
G20-2	END ROAD WORK	48"	x	24"	8
R1-2	YIELD	48"	x	48"	16
R11-2	ROAD CLOSED	48"	x	30"	10
W3-2	YIELD AHEAD	48"	x	48"	16
W4-1R	MERGING TRAFFIC	48"	x	48"	16
W4-2L	LEFT LANE ENDS SYMBOL	48"	x	48"	16
W4-2R	RIGHT LANE ENDS SYMBOL	48"	x	48"	16
W8-11	UNEVEN LANES	48"	x	48"	16
W8-15	GROOVED PAVEMENT	48"	x	48"	16
W8-15P	MOTORCYCLE PLAQUE	30"	x	24"	5
W20-1	ROAD WORK AHEAD	48"	x	48"	16
W20-1	ROAD WORK 1 MILE	48"	x	48"	16
W20-1	ROAD WORK 1/2 MILE	48"	x	48"	16
W20-1	ROAD WORK 1000 FEET	48"	x	48"	16
W20-5L	LEFT LANE CLOSED 1/2 MILE	48"	x	48"	16
W20-5L	LEFT LANE CLOSED 1500 FEET	48"	x	48"	16
W20-5L	LEFT TWO LANES CLOSED 1/2 MILE	48"	x	48"	16
W20-5L	LEFT TWO LANES CLOSED 1500 FEET	48"	x	48"	16
W20-5R	RIGHT LANE CLOSED 1/2 MILE	48"	x	48"	16
W20-5R	RIGHT LANE CLOSED 1500 FEET	48"	x	48"	16
W20-5R	RIGHT TWO LANES CLOSED 1/2 MILE	48"	x	48"	16
W20-5R	RIGHT TWO LANES CLOSED 1500 FEET	48"	x	48"	16
W21-2	FRESH OIL	48"	x	48"	16
				TOTAL	2984

APPLIED TEXTURE FINISH (EXISTING STRUCTURES)				
ITEM NO. 604-04.02	START L.M.	STOP L.M.	LENGTH	S.Y.
51" MEDIAN WALL	21.10	25.84	4.74	25491.6
	26.52	27.35	0.83	4463.7
36" MEDIAN WALL	25.84	26.52	0.68	2725.9
	27.35	27.84	0.49	1964.3
36" OUTSIDE WALL / PARAPET	21.53	21.57	0.04	160.3
	23.17	23.21	0.04	160.3
	23.93	23.95	0.02	80.2
	24.54	24.72	0.18	721.6
			TOTAL	35767.9

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-24-9(97)	2E

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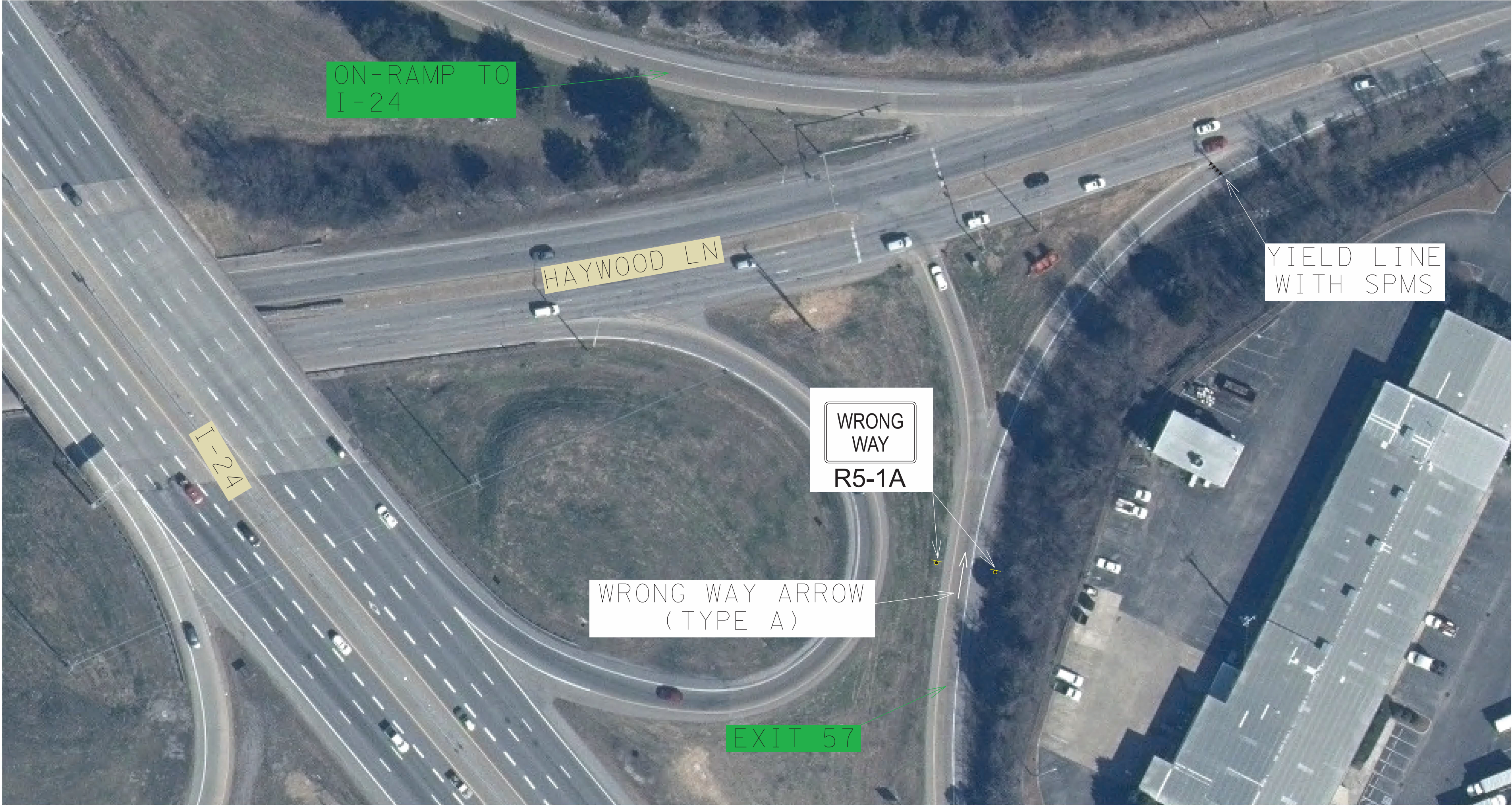
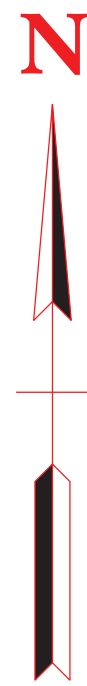


1/6/2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TABULATED
QUANTITIES

\$\$\$\$\$TIME\$\$\$\$\$
\$\$\$\$\$DGN\$PEC\$\$\$\$\$



INTERSTATE 24
LOG MILE 21.56 - 21.67
DAVIDSON COUNTY

REFERENCE STANDARD DRAWING
T-M-9, T-M-9B

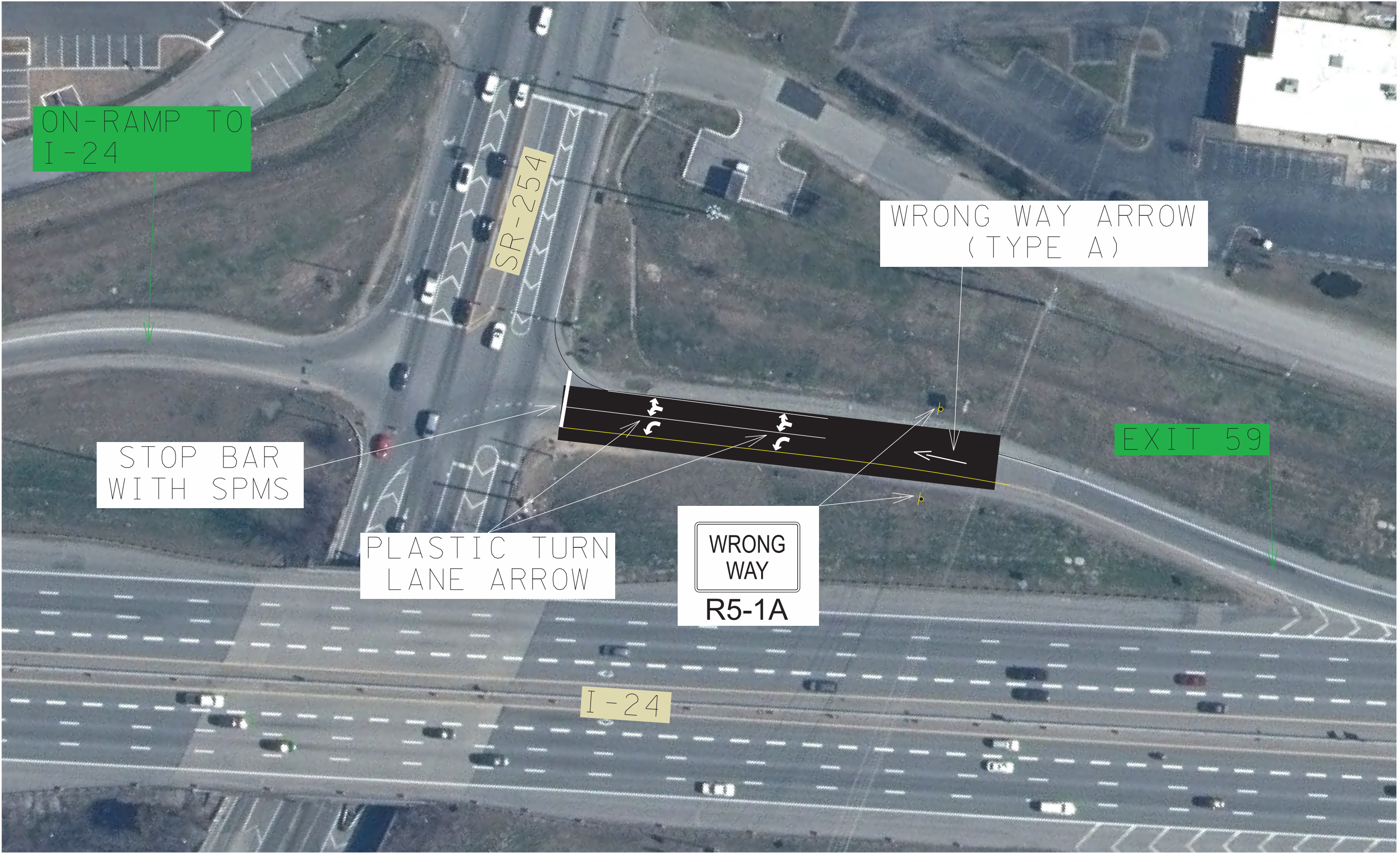
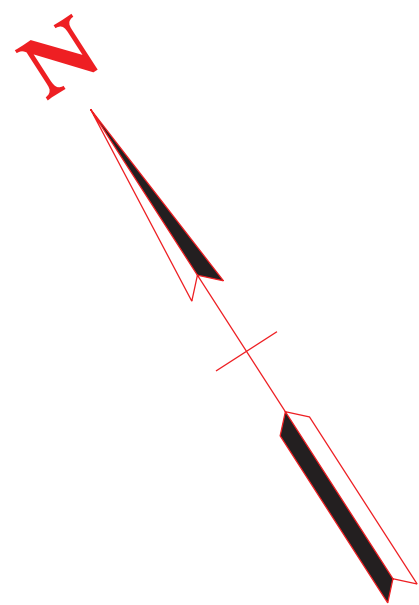
FOR REFERENCE ONLY
PROVIDED TDOT REGION 3 TRAFFIC

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-24-9(97)	2F

SEALED BY

STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION
PAVEMENT MARKING IMPROVEMENTS
NOT TO SCALE

\$\$\$\$\$TIME\$\$\$\$\$
\$\$\$\$\$DGN\$PEC\$\$\$\$\$



INTERSTATE 24
LOG MILE 23.92 - 24.03
DAVIDSON COUNTY

REFERENCE STANDARD DRAWING
T-M-9, T-M-9B

FOR REFERENCE ONLY
PROVIDED TDOT REGION 3 TRAFFIC

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-24-9(97)	2F2

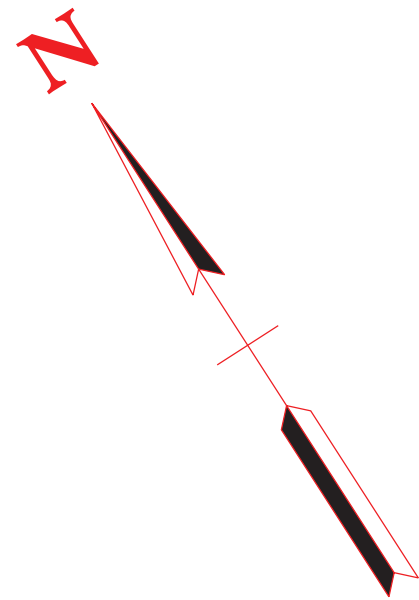
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STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION
PAVEMENT MARKING IMPROVEMENTS
NOT TO SCALE

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-24-9(97)	2F3



\$\$\$\$\$TIME\$\$\$\$\$
\$\$\$\$\$DGN\$PEC\$\$\$\$\$



INTERSTATE 24
LOG MILE 24.15 - 24.44
DAVIDSON COUNTY

REFERENCE STANDARD DRAWING
T-M-6

FOR REFERENCE ONLY
PROVIDED TDOT REGION 3 TRAFFIC

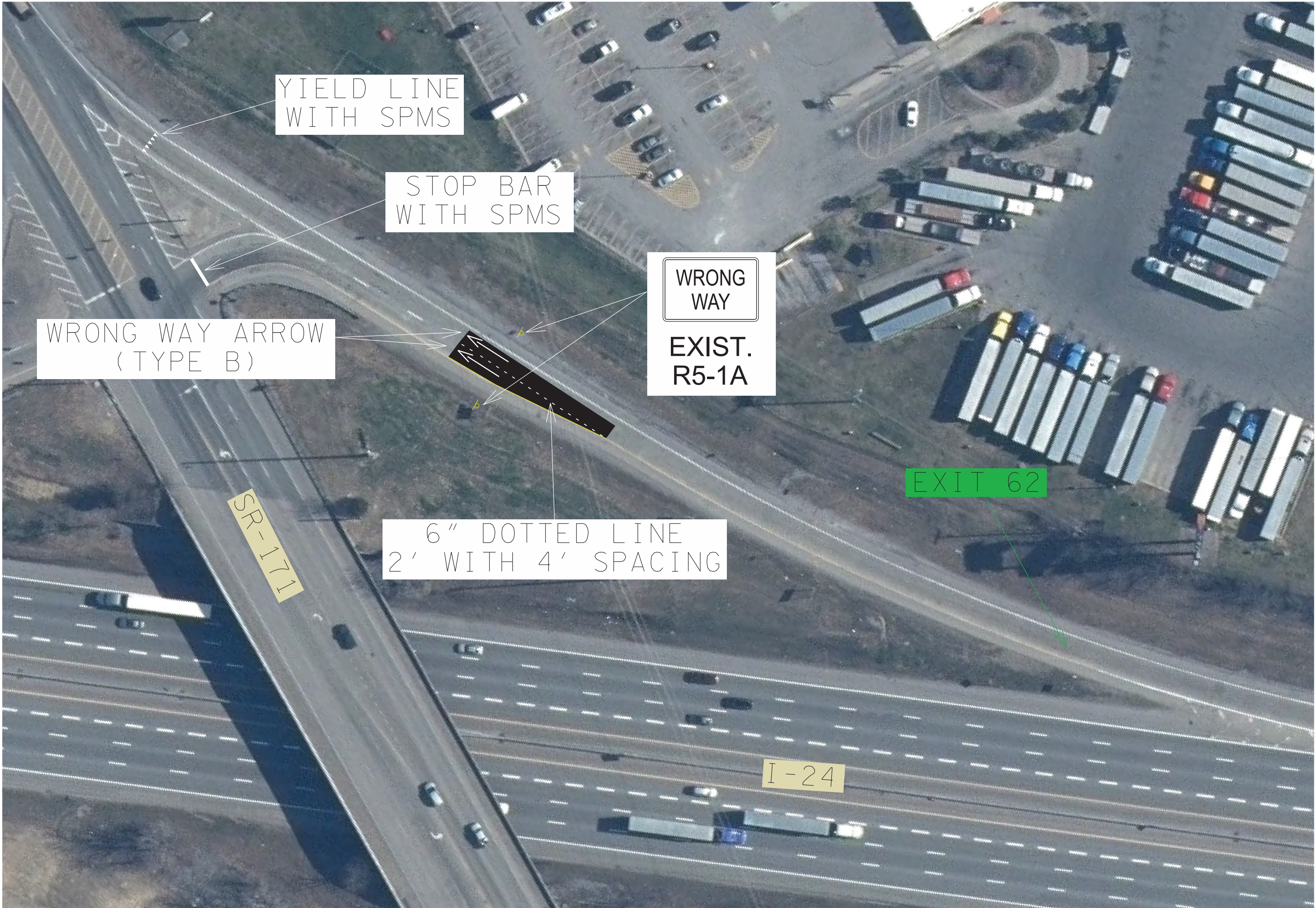
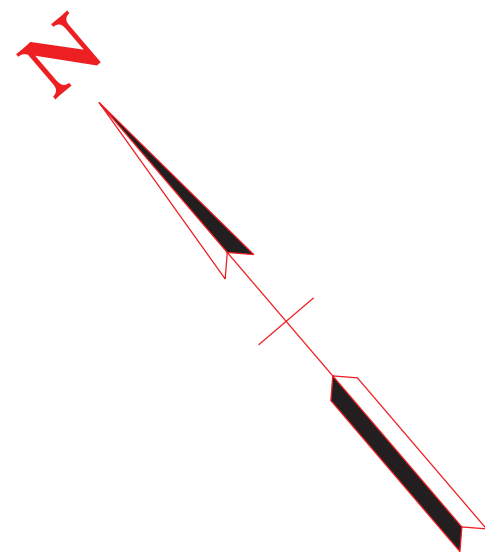
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT
MARKING
IMPROVEMENTS

NOT TO SCALE

\$\$\$\$\$TIMES\$\$\$\$\$
\$\$\$\$\$DONSPEC\$\$\$\$\$



ESTIMATED PAVEMENT MARKING QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
713-16.20	SIGNS (R5-1A)	EACH	4
716-01.22	SNOWPLOWABLE RAISED PAVEMENT MARKERS (MONO-DIR)(1 COLOR)	EACH	28
716-01.23	SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR)(2 COLOR)	EACH	58
716-02.05	PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	54
716-02.06	PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	2
716-04.02	PLASTIC PAVEMENT MARKING (DOUBLE TURNING ARROW)	EACH	2
716-04.06	PLASTIC PAVEMENT MARKING (WRONG WAY ARROW)	EACH	2
716-04.07	PLASTIC PAVEMENT MARKING (EXIT ONLY ARROW)	EACH	3
716-04.12	PLASTIC PAVEMENT MARKING (YIELD LINE)	S.F.	27
716-12.05	ENHANCED FLATLINE THERMO PVMT MRKNG (6IN DOTTED LINE)	L.F.	509

INTERSTATE 24
LOG MILE 26.96 - 27.10
DAVIDSON COUNTY

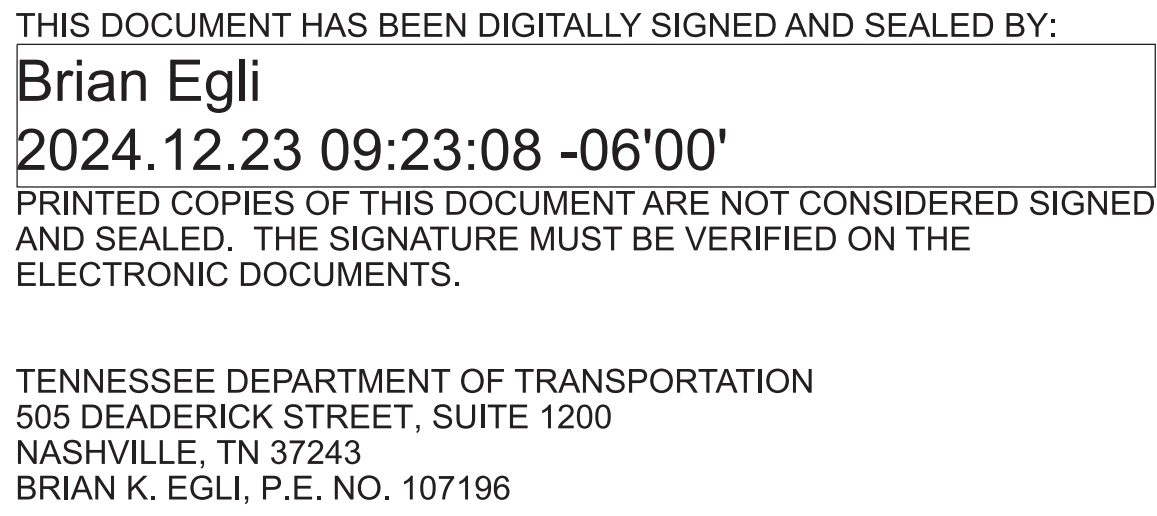
REFERENCE STANDARD DRAWING
T-M-9, T-M-9B

FOR REFERENCE ONLY
PROVIDED TDOT REGION 3 TRAFFIC

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-24-9(97)	2F4

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STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION
PAVEMENT MARKING IMPROVEMENTS NOT TO SCALE



THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET _____ BRIDGE PLANS _____	STRUCTURE-SIGN 1 B1 THRU B11

TENNESSEE DEPARTMENT OF TRANSPORTATION
505 DEADERICK STREET, SUITE 1200
NASHVILLE, TN 37243
BRIAN K. EGLI, P.E. NO. 107196

YEAR	PROJECT NO.	SHEET NO.
2025	19I024-M3-013	STRUCTURE-SIGN 1

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE SHEET

\$\$\$\$SYTIME\$\$\$\$
\$\$\$\$GDSPEC\$\$\$\$

\$\$\$\$\$YTIME\$\$\$\$\$
\$\$\$\$\$DGN\$SPEC\$\$\$\$\$

PIN NO.:131211.00

DESIGN BY:DATE: / /

DRAWN BY: SILESHI ERGICHODATE: 10/24

SUPERVISED BY: KEVIN MARTINKODATE: 10/24

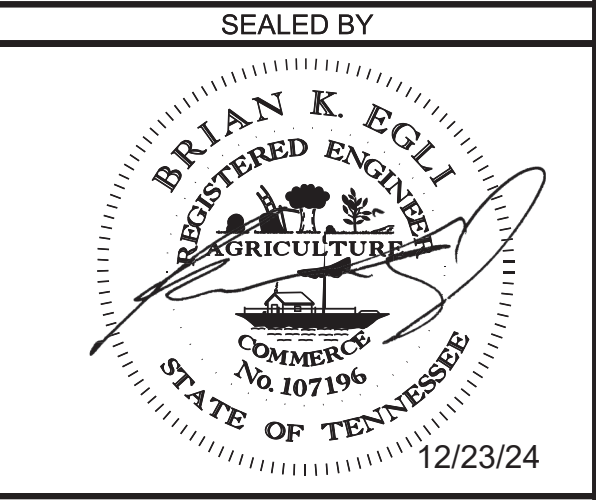
CHECKED BY:DATE: / /

<u>INDEX OF DRAWINGS</u>	<u>DWG. NO.</u>	<u>LAST REV. DATE</u>
INDEX OF DRAWINGS	B1	
BRIDGE TABULATION & ESTIMATED QUANTITIES	B2	
REPAIRS NOTES AND DETAILS	B3	
PALN VIEW REPAIRS LOCATIONS	B4	
PHASE CONSTRUCTION	B5	
PALN VIEW REPAIRS LOCATIONS	B6	
PHASE CONSTRUCTION	B7	
PALN VIEW REPAIRS LOCATIONS	B8	
PHASE CONSTRUCTION	B9	
TYPE 1 THIN EPOXY OVERLAY NOTES	B10	
EXPANSION JOINT DETAILS AT APPROACHES		
PAVEMENTS ENDS	B11	

<u>INDEX OF REFERENCE DRAWINGS</u>	<u>DWG. NO.</u>
LAYOUT OF THE BRIDGE	M-392-102
SUPERSTRUCTURE	M-392-104
LAYOUT OF THE BRIDGE	BR-130-896
BRIDGE REPAIRS DETAILS	BR-130-899
BRIDGE REPAIRS DETAILS	BR-130-904
ELEVATION OF BRIDGE	M-353-53
PLAN OF BRIDGE	M-353-54
PHASE CONSTRUCTION DETAILS	M-353-59
REINFORCED CONCRETE PAVEMENT AT THE BRIDGE ENDS	STD-1-5
REINFORCED CONCRETE PAVEMENT AT THE BRIDGE ENDS	K-86-144

FEDERAL PROJECT # NH-I-24-9(97)

PROJECT NO.		YEAR	SHEET NO.
19I024-M3-013		2025	B-1
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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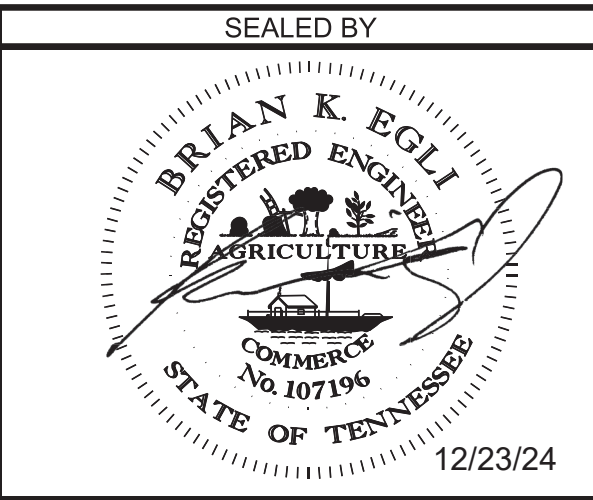


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
INDEX OF DRAWINGS
19-I24-21.53 OVER
HAYWOOD LANE,
19-I24-23.17 OVER
MILL CREEK &
19-I24-23.92 OVER
BELL ROAD
DAVIDSON COUNTY
2025

PROJECT NO.		YEAR		SHEET NO.	
19I024-M3-013		2025		B-2	
REVISIONS					
NO.	DATE	BY	BRIEF DESCRIPTION		
1	- -				
	- -				
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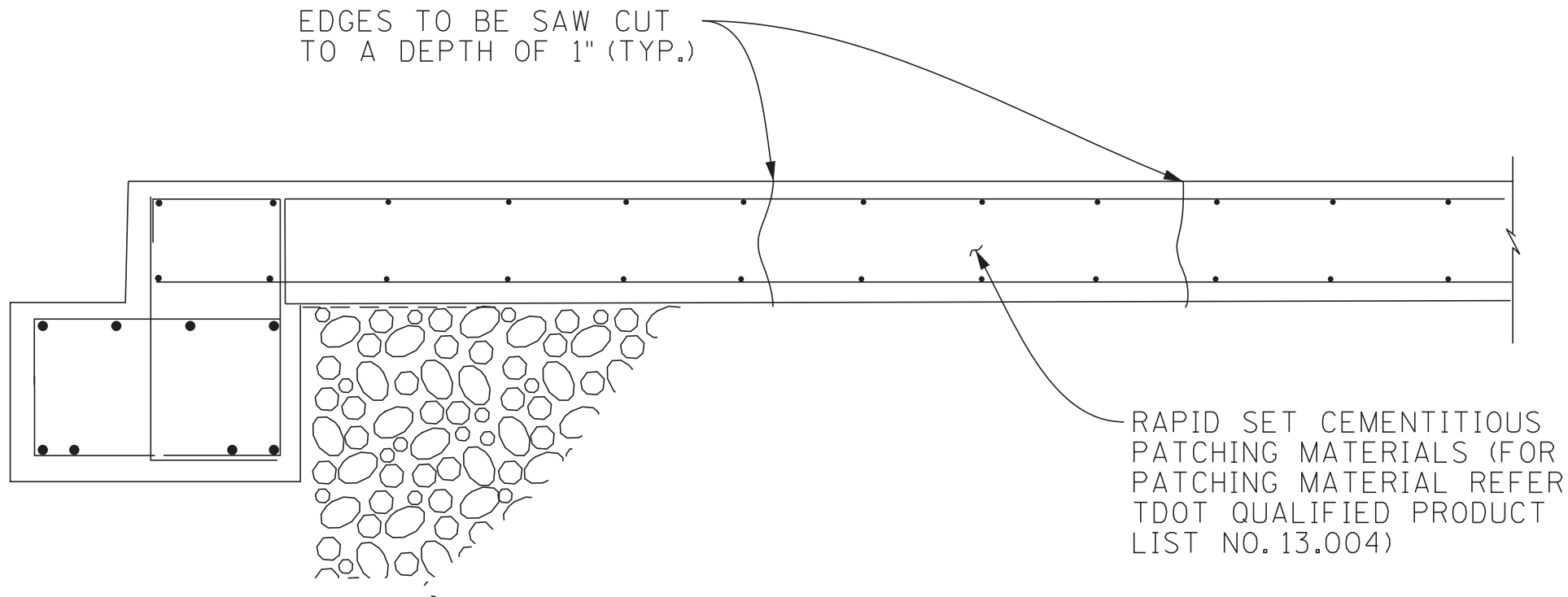
TABULATION OF BRIDGE RELATED WORK AND ESTIMATED QUANTITIES							
LOCATION OF BRIDGE AND BRIDGE NUMBER	REFERENCE DRAWINGS TO BE PRINTED WITH CONTRACT DRAWINGS	TYPE OF WORK	604-10.50 BRIDGE DECK REPAIRS (PARTIAL DEPTH OF SLAB) S.Y.	604-10.44 EXPANSION JOINT REPAIRS L.F.	604-10.53 CONCRETE REPAIRS (PARTIAL DEPTH OF APPROACH PAVEMENT) S.Y.	604-10.11 CONCRETE REPAIRS (FULL DEPTH OF APPROACH PAVEMENT) S.Y.	617-04.01 TYPE 1 THIN EPOXY OVERLAY (EPOXY-URETHANE) S.Y.
19-124-21.53 OVER HAYWOOD LANE (19I00240033)	M-392-102 M-392-104 K-86-144	BRIDGE DECK REPAIRS CONCRETE REPAIR TYPE 1 THIN EPOXY OVERLAY	10		10	50	3917
19-124-23.17 OVER MILL CREEK (19I00240039)	BR-130-896 BR-130-899 BR-130-904	TYPE 1 THIN EPOXY OVERLAY					3000
19-127-23.92 OVER BELL ROAD (19I00240041)	M-353-53 M-353-54 M-353-59 STD-1-5	BRIDGE DECK REPAIRS EXPANSION JOINT REPAIRS CONCRETE REPAIR TYPE 1 THIN EPOXY OVERLAY	10	272	5		2303
TOTAL			20	272	15	50	9220

PIN NO.:	131211.00	
DESIGN BY:		DATE: / /
DRAWN BY:	SILESHI ERGICHO	DATE: 10/24
SUPERVISED BY:	KEVIN MARTINKO	DATE: 10/24
CHECKED BY:		DATE: / /



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BRIDGE TABULATION &
ESTIMATED QUANTITIES
19-I24-21.53 OVER
HAYWOOD LANE,
19-I24-23.17 OVER
MILL CREEK &
19-I24-23.92 OVER
BELL ROAD
DAVIDSON COUNTY
2025

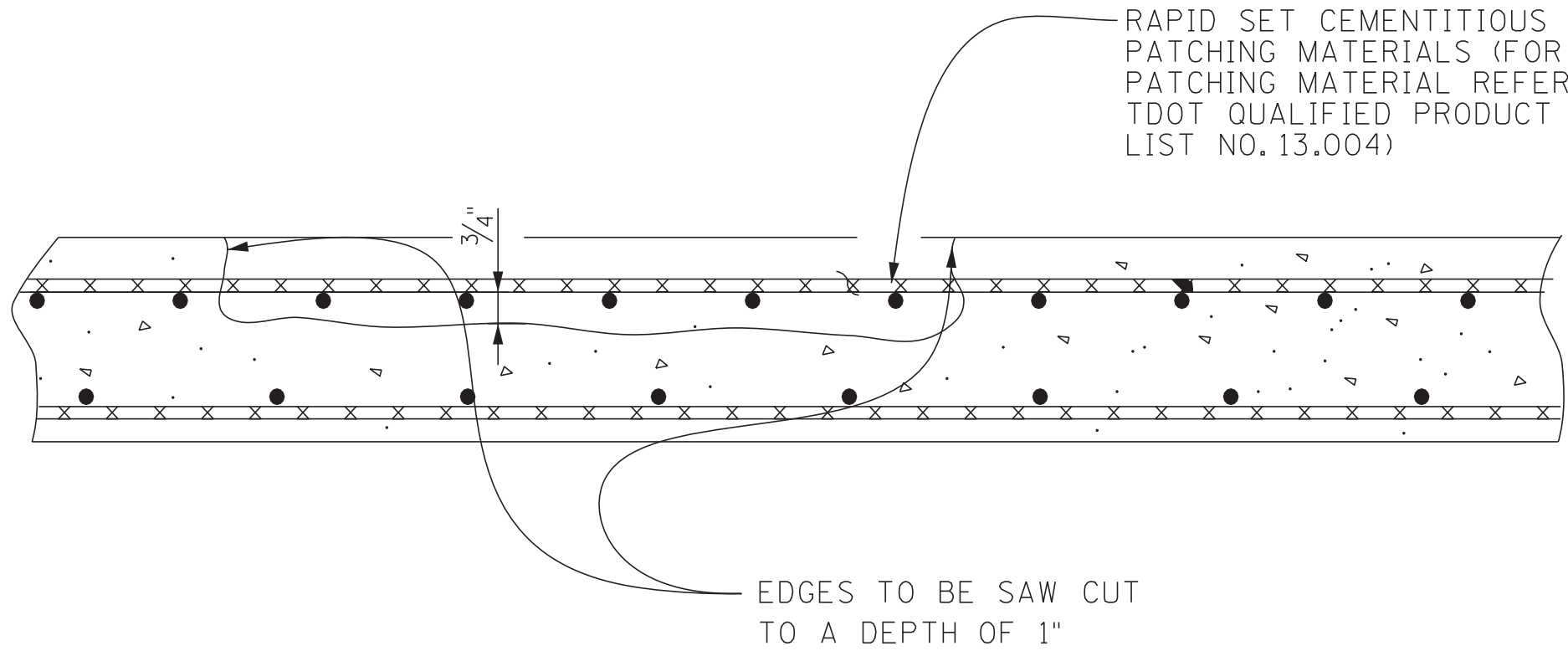
PROJECT NO.		YEAR	SHEET NO.
19I024-M3-013		2025	B-3
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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	- -		
	- -		
	- -		



CONCRETE APPROACH PAVEMENT REPAIR DETAILS FULL DEPTH:

NOTE: ITEM NO. 604-10.11 SHALL BE USED FOR REINFORCCED CONCRETE PAVEMENT AT THE BRIDGES ENDS SLAB FULL DEPTH REPAIRS. COST OF SAW CUTTING, CONCRETE REMOVAL, COMPLETELY CLEANING EXPOSED REINFORCING STEEL, LABOUR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO COMPLETE THE REPAIRS AS SHOWN SHALL ALSO BE INCLUDED IN ITEM NO. 604-10.11 CONCRETE SLAB REPAIRS S.Y.

WHEN REMOVING CONCRETE AND ASPHALT FROM THE LIMITS SHOWN, CARE SHALL BE TAKEN NOT TO DAMAGE EXISTING REINFORCING STEEL. EXISTING REINFORCING STEEL THAT IS EXPOSED DURING REMOVAL OF PORTIONS OF PABE SHALL BE THOROUGHLY CLEANED AND MAINTAINED. ANY DAMAGE TO EXISTING REINFORCING STEEL DURING CONCRETE REMOVAL SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.



SKETCH SHOWING DECK REPAIR

NOTE: PARTIAL DEPTH DECK REPAIR ONLY REMOVE CONCRETE IN ALL DELAMINATED AREAS TO A DEPTH OF ¾" BELOW THE TOP BAR OF THE TOP MAT OF REINFORCING STEEL.

NOTE: PARTIAL DEPTH REPAIRS
ALL REINFORCING STEEL IN THE AREAS OF THE DECK REPAIRS SHALL BE COMPLETELY CLEANED. CLEANING SHALL BE DONE PRIOR TO PLACING NEW CONCRETE. AREAS OF CONCRETE REMOVAL SHALL BE DESIGNATED BY PERSONNEL FROM THE HEADQUARTERS, BRIDGE INSPECTION AND REPAIR OFFICE. INSPECTIONS TO DETERMINE AREAS OF DECK REPAIR SHALL BE SCHEDULED WITH THE BRIDGE REPAIR OFFICE AT LEAST (3) DAYS IN ADVANCE.

THE COST OF CONCRETE, CONCRETE REMOVAL, LABOR, CLEANING REBAR AND ANY MISCELLANEOUS MATERIAL REQUIRED SHALL BE PAID FOR UNDER
ITEM NO. 604-10.50 BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB), S.Y.

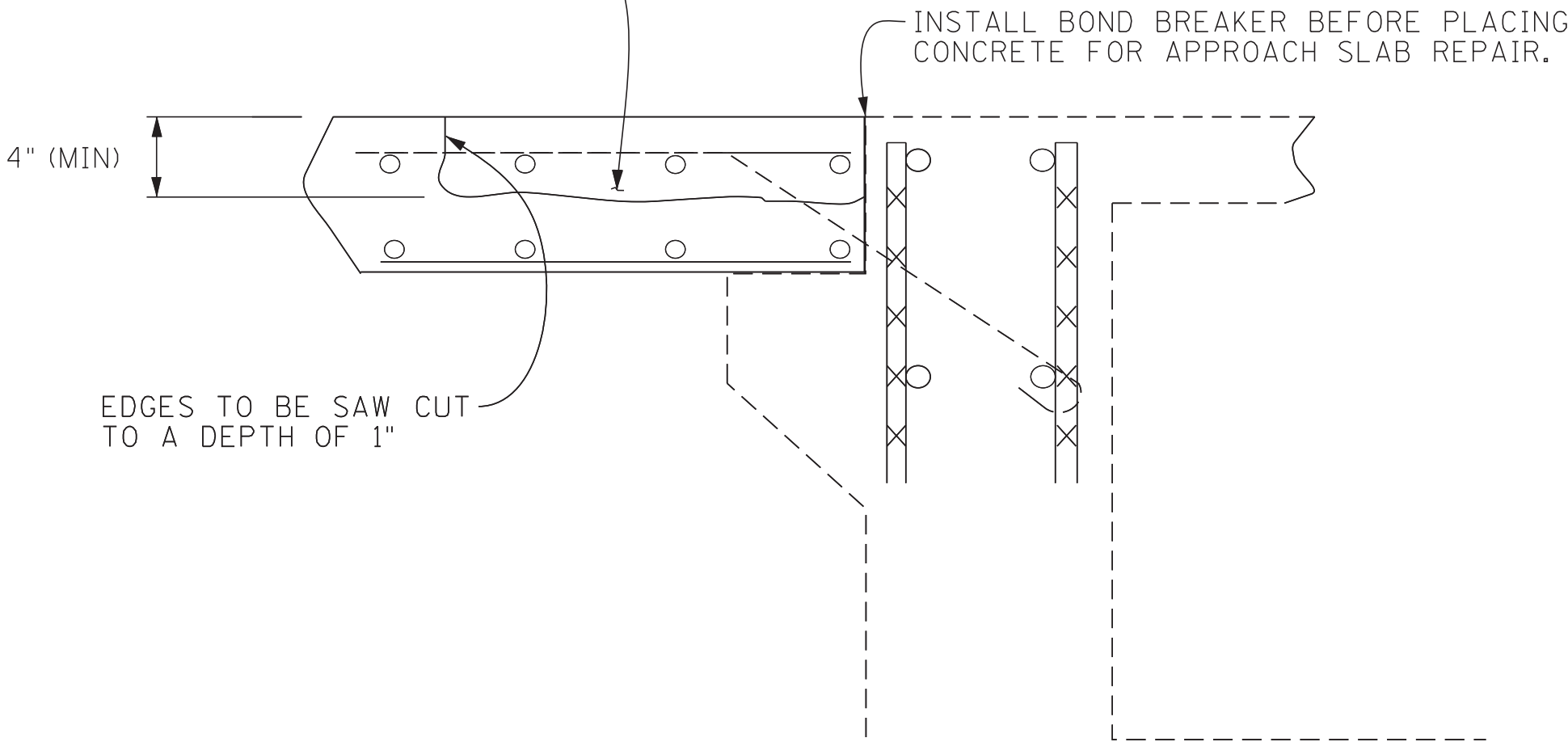
NOTE: ITEM NOS. 604-10.50, BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB) SHALL BE BID THIS MAY BE INCREASED, DECREASED OR ELIMINATED AS APPROVED BY THE ENGINEER.

BRIDGE DECK REPAIR MATERIAL

ALL REPAIRS SHALL USE ONLY NON-MAGNESIUM PHOSPHET QUICK SET PATCHING MATERIAL FROM THE TDOT QUALIFIED PRODUCTS LIST: 13.004-RAPID SET CEMENTITIUOS PATCHING MATERIALS. SHALL BE APPLIED ACCORDING TO MANUFACTURER'S SPECIFICATIONS.

PATCHING MATERIAL SHOULD BE ABLE TO ACHIEVE A MINIMUM 3000PSI AT 18 HOURS. PATCHING MATERIAL SHALL REACH 3000PSI BEFORE OPENING TO THE TRAFFIC.

SEE BRIDGE DECK REPAIR MATERIAL NOTE BELOW

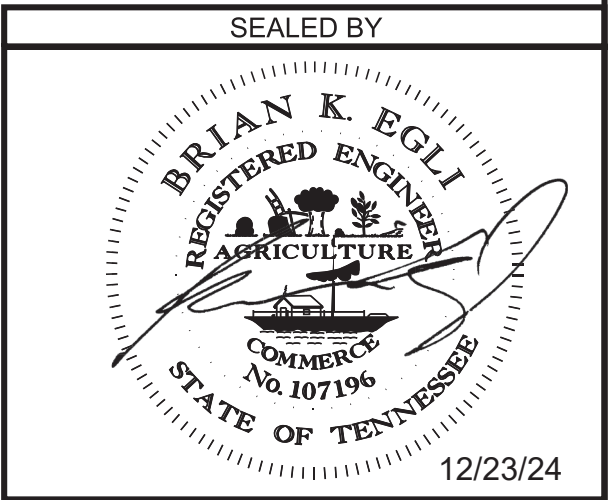


CONCRETE APPROACH PAVEMENT REPAIR DETAILS:

NOTE: REMOVE CONCRETE TO A DEPTH OF ¾" MINIMUM BELOW EXISTING REINFORCING STEEL OR TO SOUND CONCRETE. CONCRETE REMOVAL SHOULD BE TO A MINIMUM DEPTH OF 4". DEPTH FOR CONCRETE REMOVAL SHOULD NOT BE MORE THAN 6".

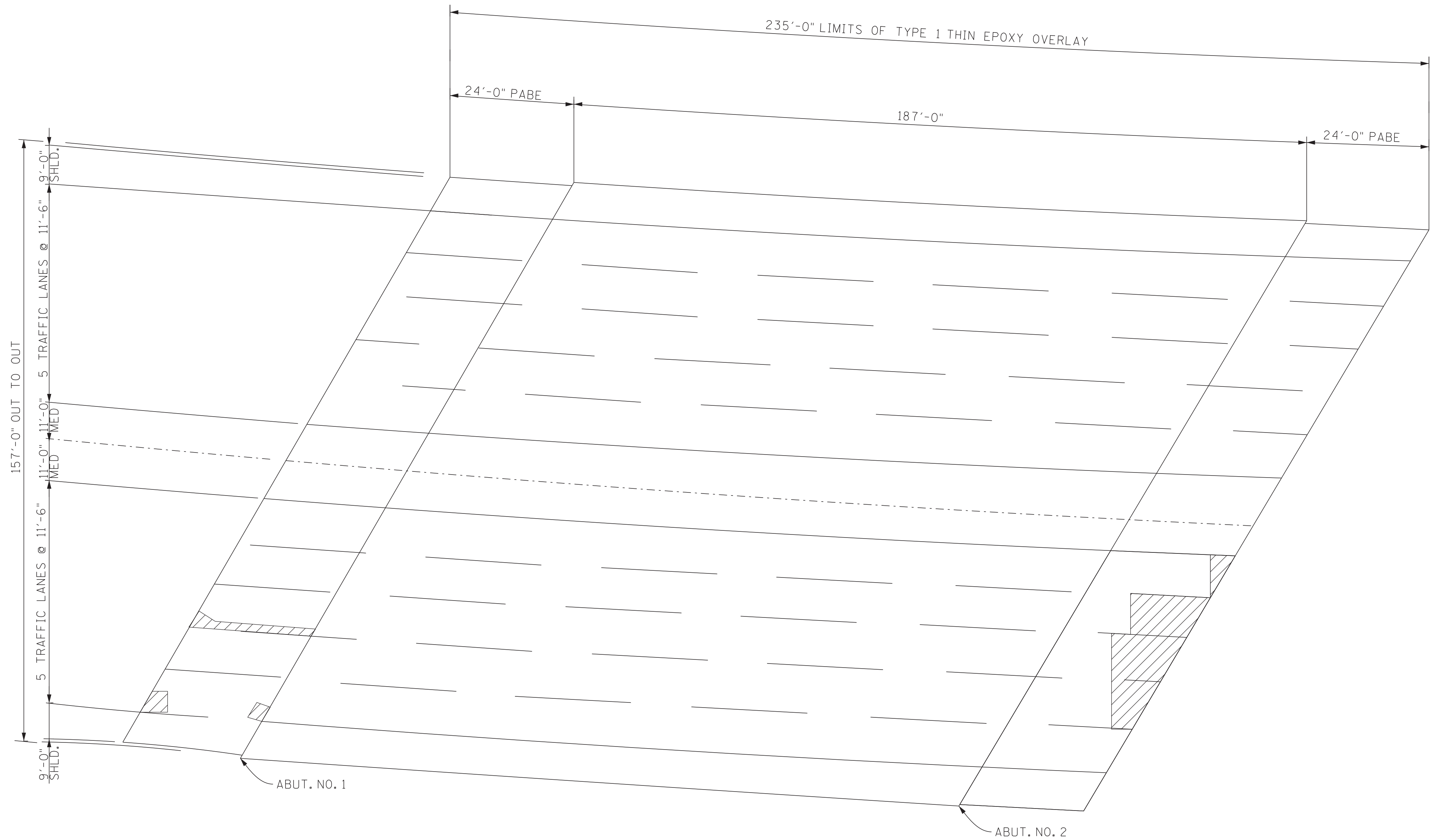
NOTE: COST OF SAW CUTTING, CONCRETE REMOVAL, COMPLETELY CLEANING EXPOSED REINFORCING STEEL, LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO COMPLETE THE REPAIRS AS SHOWN SHALL BE INCLUDED IN ITEM NO. 604-10.53 CONCRETE REPAIR S.Y.

NOTE: ITEM NO. 604-10.53, CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PYMT.) MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.

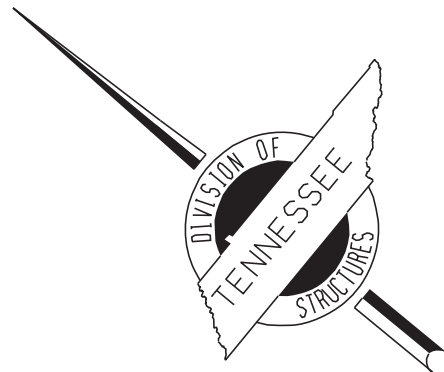


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
REPAIRS NOTES AND DETAILS
19-I24-21.53 OVER HAYWOOD LANE
& 19-I24-23.92 OVER BELL ROAD
DAVIDSON COUNTY
2025

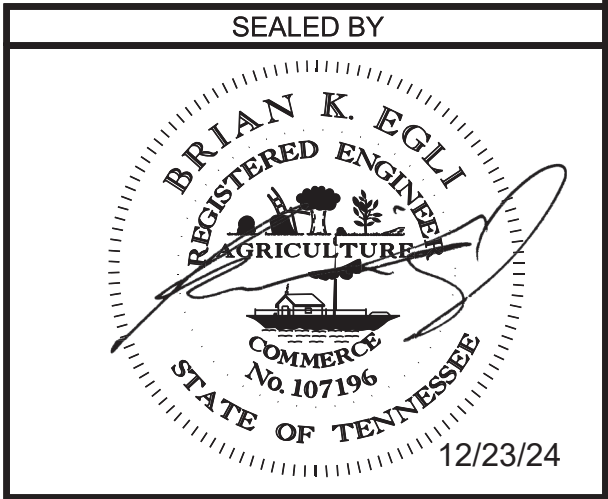
PROJECT NO.		YEAR	SHEET NO.
19I024-M3-013		2025	B-4
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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-	-		
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PLAN VIEW
19-I24-21.53



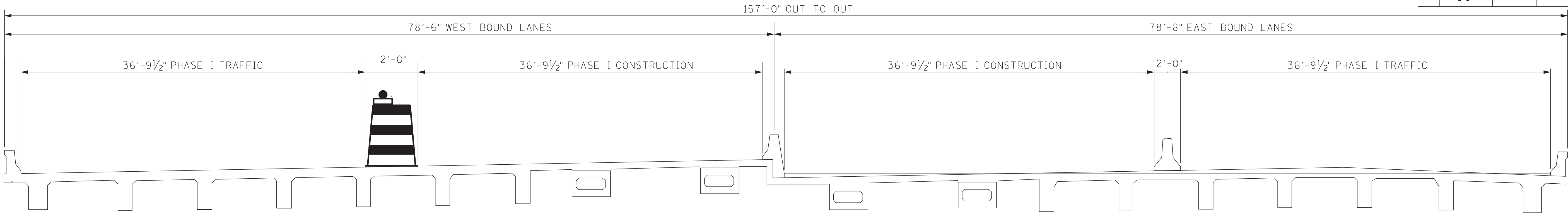
/// DENOTES: APPROXIMATE CONCRETE REPAIRS (FULL DEPTH OF APPROACHES PAVEMENT) LOCATIONS.
SEE SHEET B-3 FOR REPAIRS DETAILS



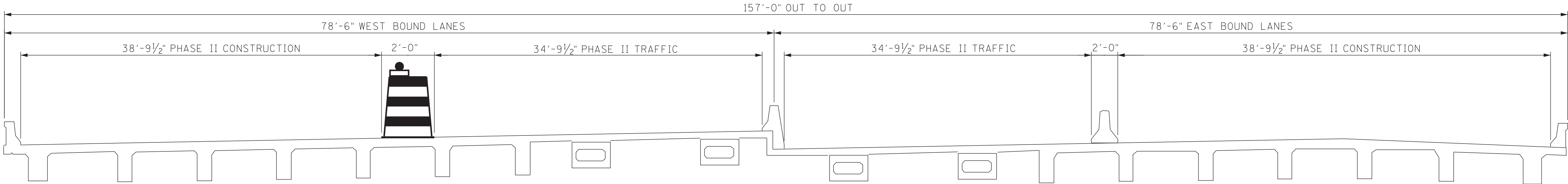
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PLAN VIEW
REPAIRS LOCATIONS
19-I24-21.53 OVER
HAYWOOD LANE
DAVIDSON COUNTY
2025

PIN NO.: 131211.00
DESIGN BY: SILESHI ERGICHO
DRAWN BY: KEVIN MARTINKO
SUPERVISED BY: KEVIN MARTINKO
CHECKED BY:
DATE: / /
DATE: 10/24
DATE: 10/24
DATE: / /

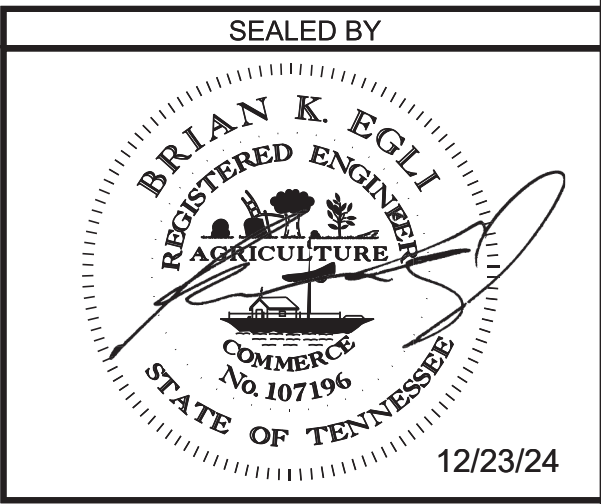
PROJECT NO.		YEAR	SHEET NO.
19I024-M3-013		2025	B-5
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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PHASE I CONSTRUCTION
(LOOKING AHEAD ON THE SURVEY)



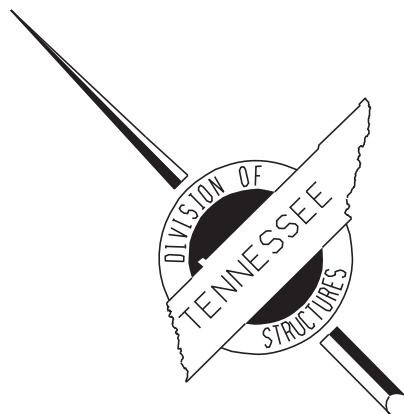
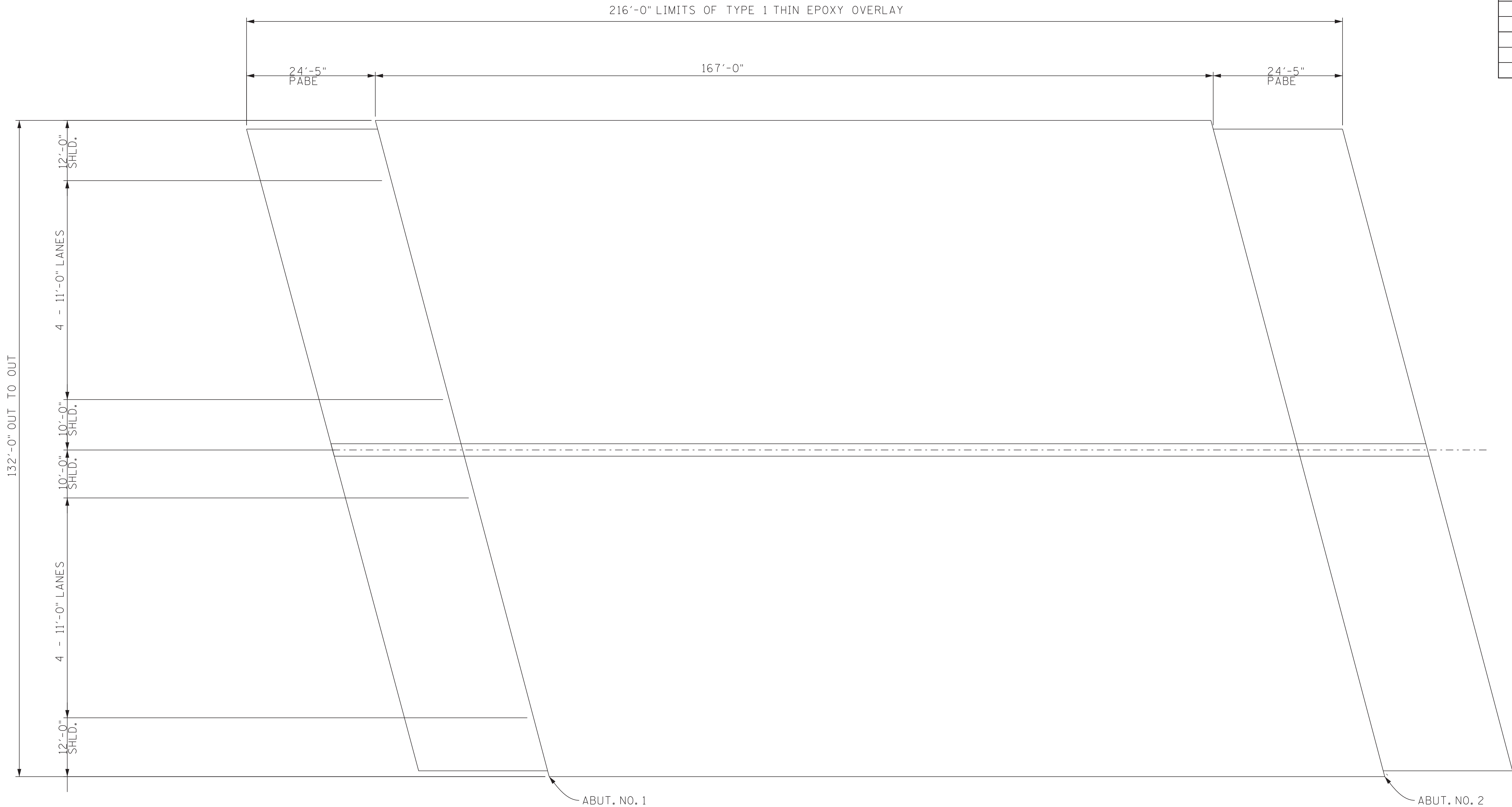
PHASE II CONSTRUCTION
(LOOKING AHEAD ON THE SURVEY)



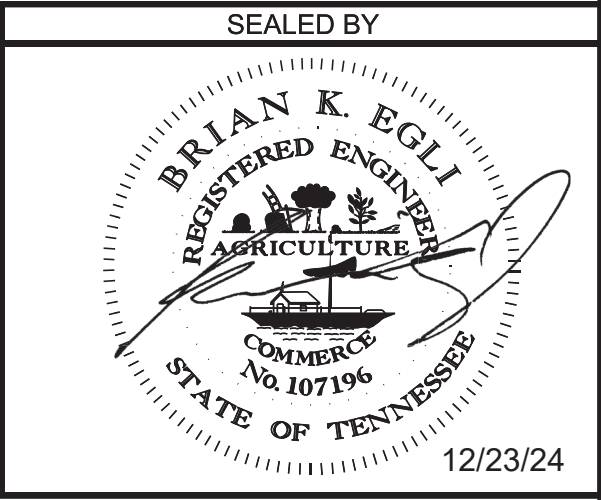
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PHASE CONSTRUCTION
19-I24-21.53 OVER
HAYWOOD LANE
DAVIDSON COUNTY
2025

PIN NO.: 131211.00
DESIGN BY: DATE: / /
DRAWN BY: SILESHI ERGICHO DATE: 10/24
SUPERVISED BY: KEVIN MARTINKO DATE: 10/24
CHECKED BY: DATE: / /

PROJECT NO.		YEAR	SHEET NO.
19I024-M3-013		2025	B-6
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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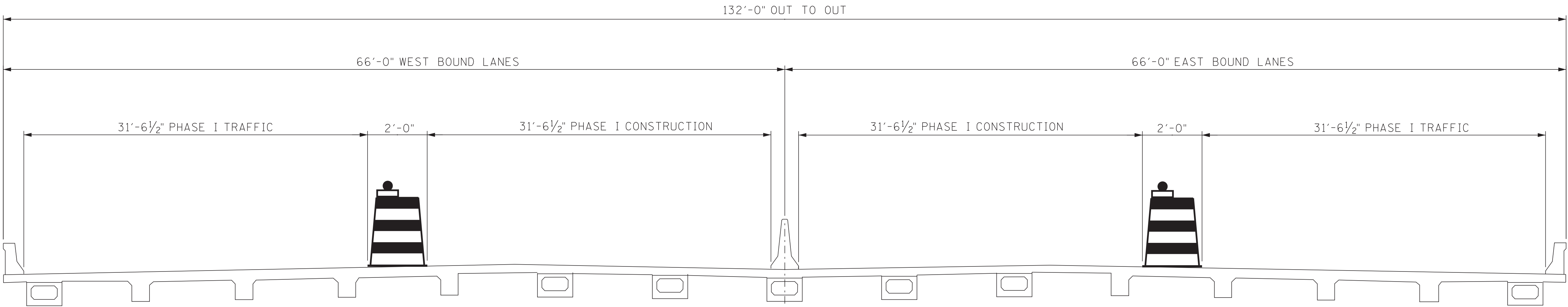
PLAN VIEW
19-124-23.17



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PLAN VIEW
REPAIRS LOCATIONS
19-124-23.17 OVER
MILL CREEK
DAVIDSON COUNTY
2025

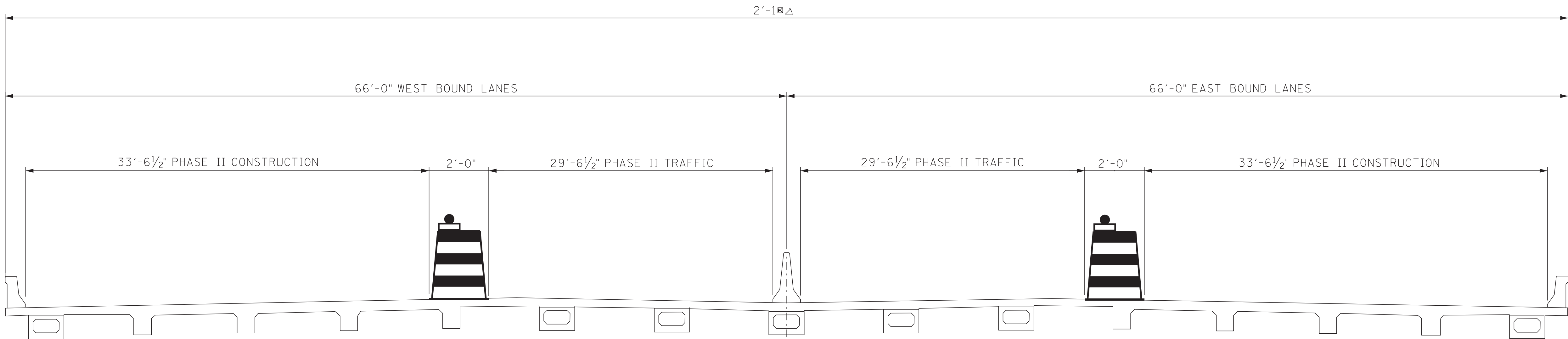
PIN NO.: 131211.00
DESIGN BY: SILESHI ERGICHO
DRAWN BY: KEVIN MARTINKO
SUPERVISED BY: KEVIN MARTINKO
CHECKED BY:
DATE: 10/24
DATE: 10/24
DATE: 10/24

PROJECT NO.		YEAR	SHEET NO.
19I024-M3-013		2025	B-7
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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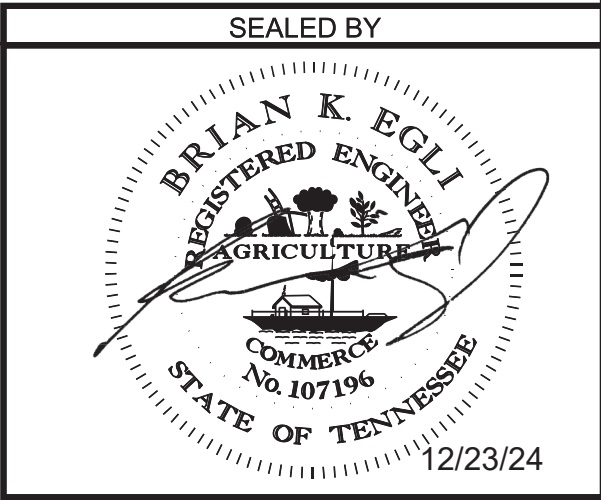
PHASE I CONSTRUCTION

(LOOKING AHEAD ON THE SURVEY)



PHASE II CONSTRUCTION

(LOOKING AHEAD ON THE SURVEY)



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PHASE CONSTRUCTION
19-I24-23.17 OVER
MILL CREEK
DAVIDSON COUNTY
2025

PIN NO.: 131211.00

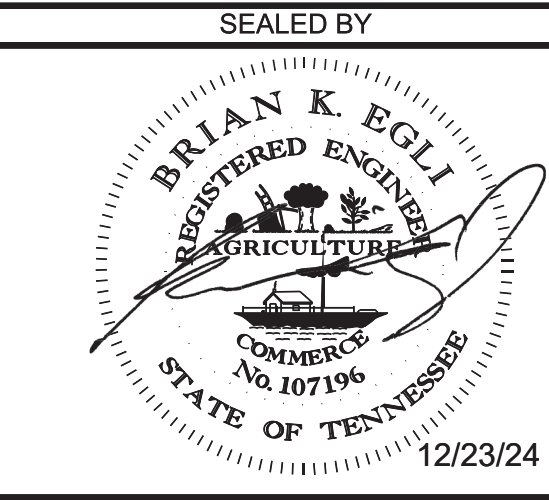
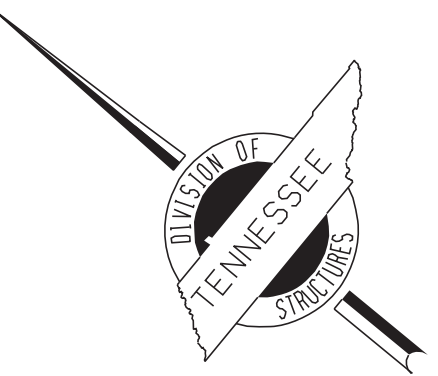
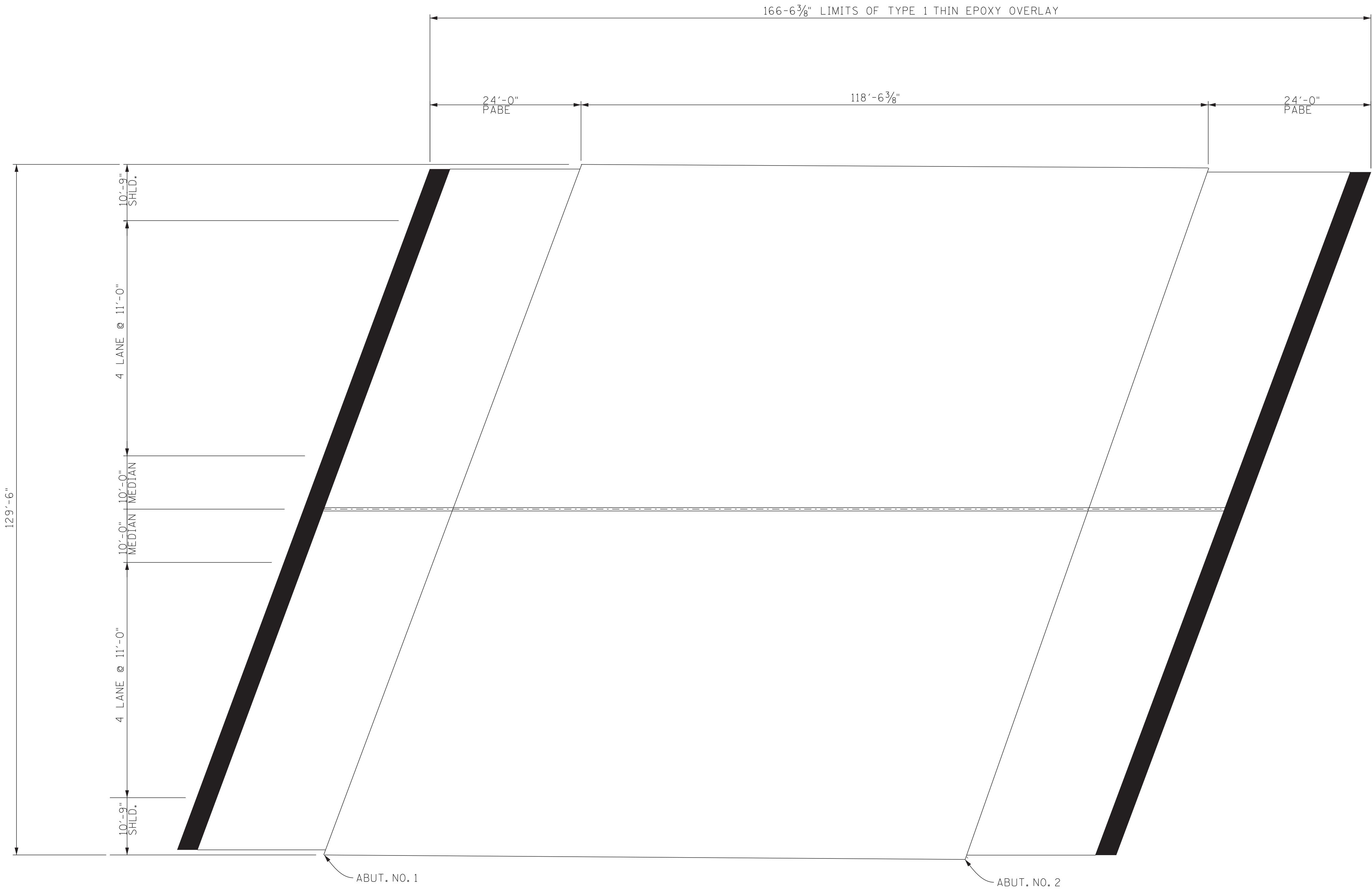
DESIGN BY: SILESHI ERGICHO DATE: 10/24

DRAWN BY: KEVIN MARTINKO DATE: 10/24

SUPERVISED BY: DATE: 10/24

CHECKED BY: DATE: 10/24

PROJECT NO.		YEAR		SHEET NO.	
19I024-M3-013		2025		B-8	
REVISIONS					
NO.	DATE	BY	BRIEF DESCRIPTION		
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PLAN VIEW
REPAIRS LOCATIONS
19-I24-23.92 OVER
BELL ROAD
DAVIDSON COUNTY
2025

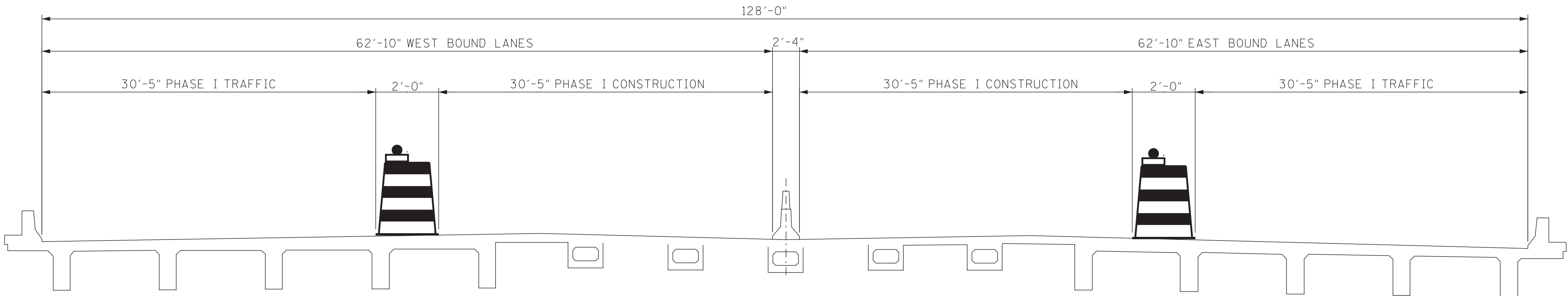
PLAN VIEW
19-I24-23.92



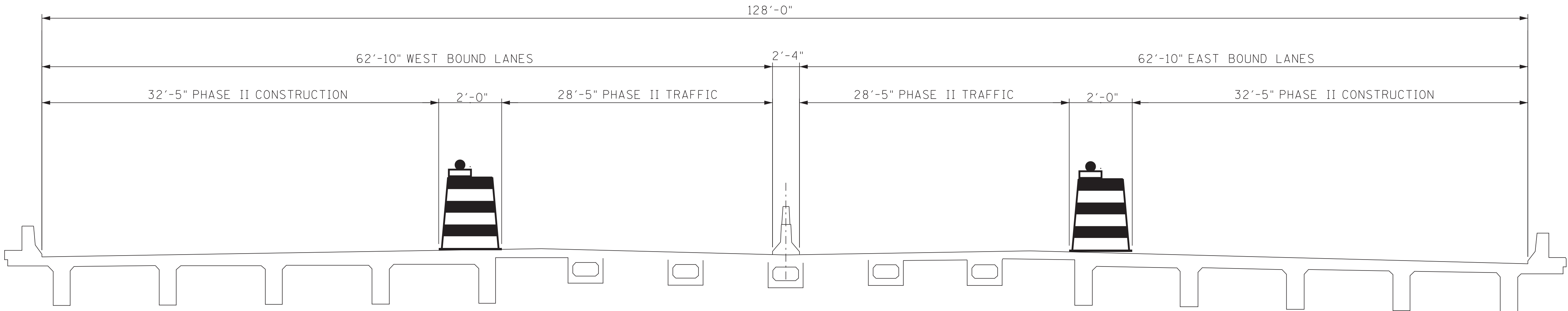
DENOTES: APPROXIMATE EXPANSION JOINT REPAIRS LOCATIONS

PIN NO.:	131211.00	DATE:	/ /
DESIGN BY:	SILESHEI ERGICHO	DATE:	10/24
DRAWN BY:	KEVIN MARTINKO	DATE:	10/24
CHECKED BY:		DATE:	/ /

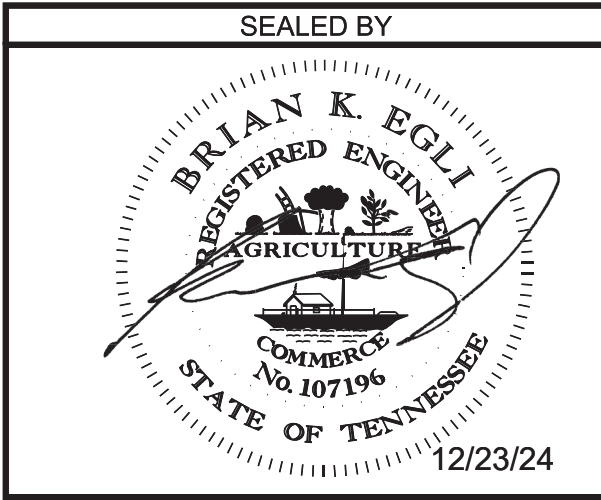
PROJECT NO.		YEAR	SHEET NO.
19I024-M3-013		2025	B-9
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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	- -		
	- -		



PHASE I CONSTRUCTION
(LOOKING AHEAD ON THE SURVEY)



PHASE II CONSTRUCTION
(LOOKING AHEAD ON THE SURVEY)



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PHASE CONSTRUCTION
19-I24-23.92 OVER
BELL ROAD
DAVIDSON COUNTY
2025

PIN NO.: 131211.00
DESIGN BY: SILESHI ERGICHO
DRAWN BY: KEVIN MARTINKO
SUPERVISED BY: KEVIN MARTINKO
CHECKED BY:
DATE: 10/24
DATE: 10/24
DATE: 10/24
DATE: 10/24

\$\$\$SYTIME\$\$\$\$\$
\$\$\$\$GNSPEC\$\$\$\$\$

PIN NO.: 131211.00
DESIGN BY: SILESHI ERGICHO
DRAWN BY: KEVIN MARTINKO
SUPERVISED BY: KEVIN MARTINKO
CHECKED BY:

DATE: / /
DATE: 10/24
DATE: 10/24
DATE: / /

TYPE 1 THIN EPOXY OVERLAY NOTES :

TYPE 1 THIN EPOXY OVERLAY SYSTEM - USE DECK PRETREATMENT/PRIMER PER MANUFACTURER'S RECOMMENDATION, AND 2 LIFTS OF AN EPOXY-URETHANE COPOLYMER AND AGGREGATE. TYPE 1 OVERLAY SHALL BE APPLIED MECHANICALLY USING METERED EQUIPMENT; HAND MIXING OF MATERIAL IS NOT PERMITTED.

THIN OVERLAY SYSTEM SHALL BE FROM THE QUALIFIED PRODUCTS LIST 23.005 TYPE 1 THIN OVERLAY (EPOXY URETHANE). MINIMUM OVERLAY THICKNESS SHALL BE 3/8 INCH.

APPLICATION EQUIPMENT SHOULD :

A) BE CAPABLE OF METERING, MIXING AND DISTRIBUTING THE POLYMER AND PRETREATMENT TO MANUFACTURER'S RECOMMENDATION.

B) USE AN APPLICATION MACHINE THAT FEATURES POSITIVE DISPLACEMENT VOLUMETRIC METERING PUMPS CONTROLLED BY A HYDRAULIC POWER UNIT.

C) STORE COMPONENTS IN TEMPERATURE CONTROLLED RESERVOIRS CAPABLE OF MAINTAINING 100 DEGREES FAHRENHEIT (PLUS OR MINUS 10 DEGREES) TO INSURE OPTIMAL MIXING.

D) CHECK MIXING RATIO AT THE PUMP OUTLETS AS WELL AS CYCLE COUNTING CAPABILITIES TO MONITOR OUTPUT ON STANDARD FEATURES.

E) USE MOTIONLESS IN-LINE MIXING SO AS TO NOT OVERLY SHEAR THE MATERIAL TO ENTRAP AIR IN THE MIX.

F) MAXIMIZE MATERIAL WORKING TIME BY MIXING IT IMMEDIATELY BEFORE DISPENSING.

AGGREGATE SHALL BE ANGULAR, HAVING LESS THAN 0.2% MOISTURE AND FREE OF DIRT, CLAY, ASPHALT AND OTHER FOREIGN OR ORGANIC MATERIALS. AGGREGATE FOR ALL LAYERS SHALL BE BAUXITE OR FLINT ROCK PRODUCTS FLINT AND MEETS THE FOLLOWING GRADATION:

SIEVE SIZE	% PASSING
NO. 6	95-100
NO. 10	10-35
NO. 20	0-3

FULL AND PARTIAL DEPTH DECK REPAIR SHALL CURE A MINIMUM OF 28 DAYS BEFORE THE OVERLAY IS PLACED. THE 28 DAYS MAY BE WAIVED IF THE OVERLAY MANUFACTURER PROVIDES A METHOD OF TESTING THE REPAIRED AREAS AND APPROVES THE PLACEMENT BY LETTER. TRAFFIC SHALL BE ALLOWED TO USE THE BRIDGE DURING THE CURING PERIOD OF THE PATCHES BUT NOT AFTER SHOTBLASTING. MAGNESIUM PHOSPHATE BASED MATERIALS WILL NOT BE ALLOWED.

THE CONCRETE DECK SURFACE SHALL BE CLEANED BY SHOTBLASTING TO REMOVE ANY OIL, DIRT, RUBBER, TRAFFIC STRIPING, OR ANY OTHER POTENTIAL DETRIMENTAL MATERIAL SUCH AS CURING COMPOUND AND LAITANCES, WHICH THE MANUFACTURER AND ENGINEER'S OPINION WOULD PREVENT PROPER BONDING AND CURING OF THE MATERIAL. IN AREAS WHERE SHOTBLASTING EQUIPMENT CAN NOT REACH (I.E., ALONG CURBS AND BRIDGE RAILS) SANDBLASTING IS PERMITTED TO AN EXTENT TO THE ENGINEER'S AND MANUFACTURER'S APPROVAL. IMMEDIATELY BEFORE APPLICATION, ALL PREPARED SURFACES SHALL BE CLEANED WITH COMPRESSED AIR OR VACUUMED TO REMOVE DUST AND DEBRIS. THE CONTRACTOR IS TO PREVENT THE TRACKING OF TACK COAT AND CONSTRUCTION DEBRIS ACROSS THE BRIDGE DECK PRIOR TO THE APPLICATION OF THE THIN OVERLAY. MILLING THE BRIDGE DECK WILL NOT BE AN OPTION FOR TACK COAT OR DEBRIS REMOVAL. REMOVAL SHALL BE AT THE CONTRACTOR'S EXPENSE.

ALL SURFACES THAT ARE TREATED SHALL BE DRY AT THE TIME OF APPLICATION. THE OVERLAY SHALL NOT BE APPLIED WHEN IT HAS RAINED 24 HOURS PRIOR TO, OR RAIN IS FORECAST WITHIN 8 HOURS AFTER, APPLICATION. THE MOISTURE CONTENT IN THE DECK SUBSTRATE SHALL BE TESTED. MOISTURE IS NOT TO EXCEED 4.5 PERCENT WHEN MEASURED BY ELECTRONIC METER. IF THE TEST SHOWS EXCESS MOISTURE, THE DECK SHALL CONTINUE TO DRY BEFORE APPLICATION PROCEEDS.

BLUSHING (A WAXY SURFACE COATING ON THE EPOXY) IS CAUSED BY THE REACTION OF MOISTURE WITH THE HARDENING AGENT. BLUSHING CREATES A SURFACE THAT MAKES FUTURE LAYERS DIFFICULT TO ADHERE. LIFTS THAT SHOW SIGNS OF BLUSHING SHALL BE REMOVED AND REPLACED PRIOR TO APPLICATION OF THE NEXT. THE COST TO REMOVE AND REPLACE THESE AREAS SHALL BE AT THE CONTRACTOR'S EXPENSE.

TRAFFIC, OTHER THAN APPLICATION EQUIPMENT, SHALL NOT BE ALLOWED ON ANY PORTION OF THE DECK THAT HAS BEEN SHOTBLASTED OR WHERE PART OF THE APPLICATION HAS BEEN PLACED.

SEE MANUFACTURER'S RECOMMENDATIONS FOR REQUIRED AMBIENT AND SURFACE TEMPERATURES AND HUMIDITY LIMITS FOR APPLICATION.

THE MANUFACTURER SHALL HAVE A REPRESENTATIVE ON THE JOB SITE AT ALL TIMES DURING APPLICATION AND CURE TIME. THE REPRESENTATIVE, ALONG WITH CONSULTATION WITH ENGINEER, MAY SUSPEND ANY ITEM OF WORK THAT IS SUSPECT AND DOES NOT MEET THE REQUIREMENTS OF THE SPECIFICATIONS. WORK SHALL NOT RESUME UNTIL THE ENGINEER AND REPRESENTATIVE ARE SATISFIED THAT APPROPRIATE REMEDIAL ACTION HAS BEEN TAKEN BY THE CONTRACTOR.

ALL COSTS FOR AGGREGATE, EPOXY FOR MINIMUM OF TWO LIFTS, SURFACE PREPARATION, LABOR AND ANY OTHER MISCELLANEOUS MATERIALS REQUIRED TO PLACE THIN OVERLAY SHALL BE INCLUDED IN ITEM NO. 617-04.01, TYPE 1 THIN EPOXY OVERLAY (EPOXY URETHANE), S.Y.

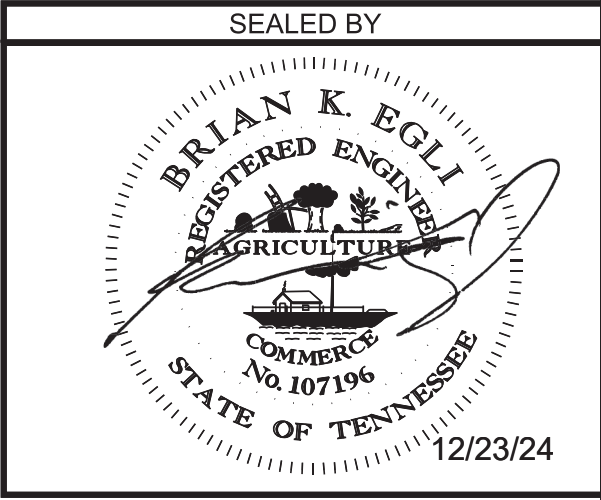
THICKNESS VERIFICATION: THE PROJECT ENGINEER SHALL BE NOTIFIED OF THE NUMBER OF GALLONS USED ON THE PROJECT WITH NOTARIZED QUANTITY STATEMENTS FROM THE CONTRACTOR AND THE MANUFACTURER. THE CONTRACTOR SHALL VERIFY TO TDOT THAT THE OVERLAY IS AN AVERAGE OF AT LEAST 3/8 INCH THICK AT THREE RANDOM LOCATIONS AGREED UPON BY THE PROJECT ENGINEER AND THE MATERIAL MANUFACTURER REPRESENTATIVE. IF 3/8 INCH AVERAGE IS NOT ACHIEVED, A RETEST SHALL BE PERFORMED IN ADJOINING AREAS. THIN AREAS SHALL BE RE-COATED AS DESCRIBED ABOVE BY THE CONTRACTOR AND RE-VERIFIED AT NO ADDITIONAL COST TO TDOT. THIS VERIFICATION MAY CONSIST OF CORES MADE BY THE CONTRACTOR WITH A CORING BIT NOT LESS THAN 1½" DIAMETER. THE TESTED AREAS SHALL BE REPAIRED BY THE CONTRACTOR BEFORE FINAL ACCEPTANCE BY THE PROJECT ENGINEER.

THE MANUFACTURER SHALL HAVE A REP ON THE JOB SITE AT ALL TIME DURING APPLICATION AND CURE TIME. THE REP WITH THE ENGINEER, MAY SUSPEND AN ITEM OF WORK THAT IS SUSPECT AND DOES NOT MEET THE REQUIREMENTS OF THE SPECS. WORK SHALL NOT RESUME UNTIL THE ENGINEER AND REP ARE SATISFIED THAT APPROPRIATE REMEDIAL ACTION HAS BEEN TAKEN BY THE CONTRACTOR.

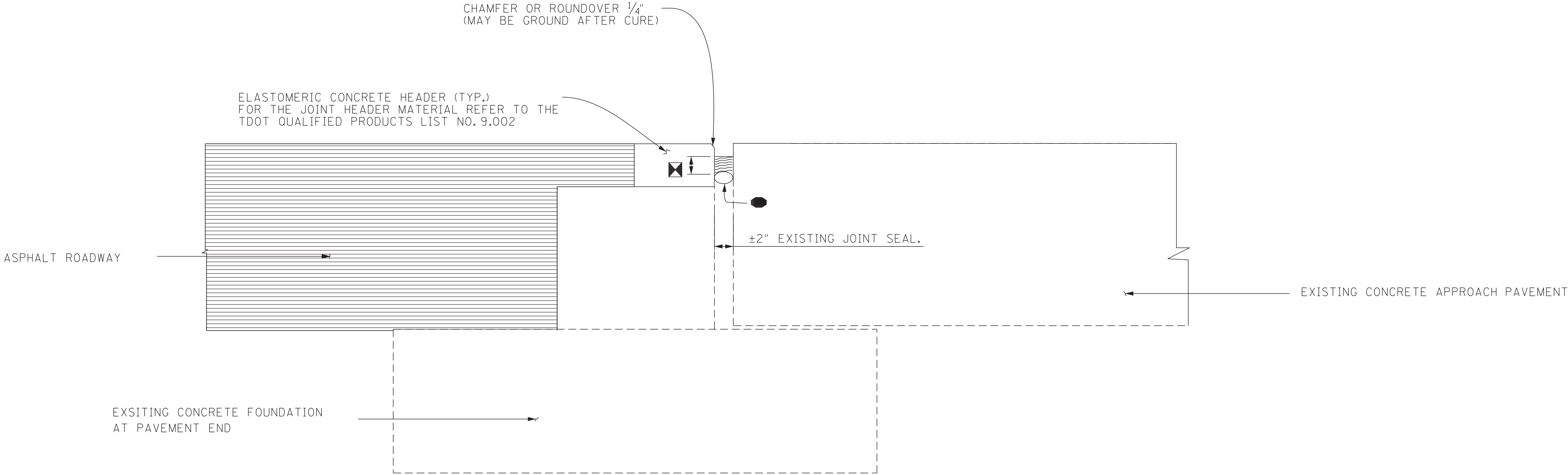
PROJECT NO.		YEAR	SHEET NO.
19I024-M3-013		2025	B-10
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
	- -		
	- -		
	- -		
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	- -		

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPE 1 THIN EPOXY
OVERLAY NOTES
19-124-21.53 OVER
HAYWOOD LANE,
19-124-23.17 OVER
MILL CREEK &
19-124-23.92 OVER
BELL ROAD
DAVIDSON COUNTY
2025



PROJECT NO.		YEAR	SHEET NO.
19I024-M3-013		2025	B-11
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
	- -		
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- NOTE: THE EXISTING JOINT OPENING SHALL BE CAULKED WITH A BACKER ROD OF SUITABLE DIAMETER,THE ROD SHALL BE PLACED AT A DEPTH TO INSURE THE CORRECT WIDTH/DEPTH RATIO OF THE NEW JOINT SEALANT MATERIAL,BACKER ROD SHALL BE AS PER JOINT MANUFACTURERS RECOMMENDATIONS.
- ☒ NOTE: FULL DEPTH OF ALL EXISTING JOINTS SHALL SHALL BE RESEALED WITH NEW JOINT SEALER. THE NEW JOINT SEALER SHALL BE A COLD PUOR TWO PART SILICONE SEALER FROM OPL 5.001.

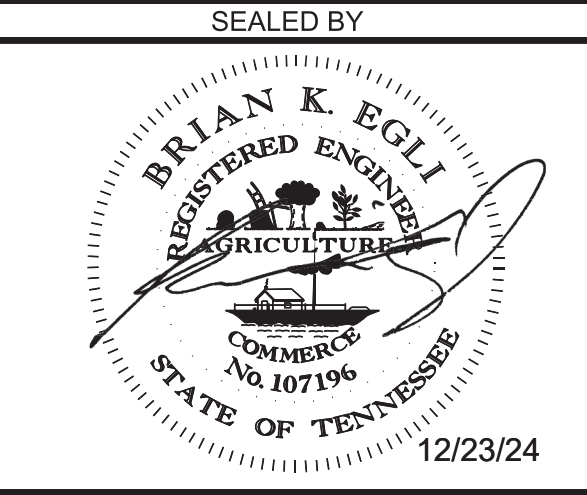
EXPANSION JOINT REPAIR NOTES:

THE JOINT HEADER SYSTEM SHALL BE FROM OPL 9.002. THE JOINT SEAL SYSTEM CONSISTS OF THE ELASTOMERIC HEADER MATERIAL AND A TWO-PART COLD POUR SILICONE SEALANT. THE CONTRACTOR SHALL ALSO HAVE THE OPTION OF USING A PRECOMPRESSED FOAM WITH SILICONE TOPPED PRODUCT (SIMILAR TO AND INCLUDING BEJS) FROM OPL 7.001 WITH AN APPROPRIATELY SIZED SEAL FOR THE JOINT OPENING, AND AN ELASTOMERIC CONCRETE FROM OPL 9.001. THE SYSTEM SHALL BE INSTALLED UNDER THE DIRECT SUPERVISION OF AN AUTHORIZED TECHNICIAN PROVIDED BY THE SYSTEM MANUFACTURER. THE TECHNICIAN MUST APPROVE ALL ASPECTS OF THE GEOMETRY AND PREPARATION OF THE JOINT LOCATIONS PRIOR TO ANY INSTALLATION OF THE JOINT SYSTEM MATERIALS. PRODUCTS FROM OPL 7.001 "ON A ROLL" AND "COMPRESSION ONLY" WILL NOT BE ALLOWED. THE TOP OF THE OPL 7.001 JOINT FILLER SHALL BE A MINIMUM OF 3/4 OF AN INCH BELOW THE ROADWAY SURFACE UNLESS THE MANUFACTURER REQUIRES A DEEPER DEPTH.

MANUFACTURERS SPECIFICATIONS AND INSTALLATION PROCEDURES SHALL BE SUBMITTED TO THE PROJECT ENGINEER FOR REVIEW PRIOR TO THE JOINT REPLACEMENT/REPAIR WORK. THE MANUFACTURER AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE WORKMANSHIP OF THE JOINT INSTALLATION.

PRIOR TO THE INSTALLATION OF THE NEW JOINT,THE EXISTING JOINT OPENING SHALL BE CLEANED OF ALL DIRT,DEBRIS, AND PRIOR CONSTRUCTION MATERIAL, ETC., THE FULL DEPTH OF THE OPENING. THE SURFACES WHERE THE NEW MATERIAL BONDS TO STAY IN PLACE, SHALL BE CLEANED PER MANUFACTURERS RECOMMENDATION, TO REMOVE ANY SUBSTANCES THAT WOULD INHIBIT BONDING.

THE COST FOR REMOVING THE OLD JOINT SYSTEM,INSTALLING THE NEW JOINT SYSTEM,LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO INSTALL THE NEW EXPANSION JOINT, IS TO BE INCLUDED UNDER ITEM NUMBER 604-10.44, EXPANSION JOINT REPAIRS, L.F.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
EXPANSION JOINT
DETAILS AT APPROACHES
PAVEMENTS ENDS
19-I24-23.92 OVER
BELL ROAD
DAVIDSON COUNTY
2025

PIN NO.: 131211.00
DESIGN BY: SILESHI ERGICHO
DRAWN BY: KEVIN MARTINKO
SUPERVISED BY: KEVIN MARTINKO
CHECKED BY:

DATE: / /
DATE: 10/24
DATE: 10/24
DATE: / /


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(INTERSTATE 24)
PI 300+40.21
N 632.976.4460
E 1,764.604.6730
Δ 13°40'24.19" LT.
D 00°59'16.29"
R 5,800.00
T 695.3757'
Lc 1,384.1446'
DES. SPEED 70 MPH
TRANS. LENGTH = 180 FT.

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[illegible]

NOTE: FINISHED GRADE ELEVATIONS ARE FROM FIELD DATA.

LIST OF STANDARD DRAWINGS	DWG. NO.	LATEST REV. DATE
*BRIDGE RAILING CONCRETE PARAPET	STD-1-1	5-21-99
*PAVEMENT AT BRIDGE ENDS	STD-1-5	9-06-99
BRIDGE END DRAIN WITH PABE	STD-1-6	4-28-97
*BRIDGE END DRAIN WITH PABE	STD-1-7	5-21-99
BRIDGE END DRAIN WITH 2' X 8'-7" WITH PABE	STD-1-8	5-01-95
BRIDGE END DRAIN 4' X 8'-7" WITH PABE	STD-1-9	5-01-95
STANDARD PILE DETAILS	STD-5-1	10-25-93
*STANDARD PILE DETAILS	STD-5-2	5-21-99
*STANDARD SEISMIC DETAILS	STD-6-1	5-21-99
REINFORCING BAR SUPPORT DETAILS FOR CONCRETE SLAB	STD-9-1	12-19-94
MISCELLANEOUS ABUTMENT AND DRAINAGE DETAILS	STD-10-1	5-11-92
SLOPE PROTECTION	RD-SA-1	5-27-98

*DENOTES PRINT WITH PLANS.

LIST OF SPECIAL PROVISIONS	PROV. NO.	LATEST REV. DATE
REGARDING APPROVAL OF SHOP DRAWINGS.....	105A	12-15-97

LIST OF DRAWINGS	DWG. NO.	REV. DATE
LAYOUT.....	M-392-102	3-29-00
GENERAL NOTES AND		
ESTIMATED QUANTITIES.....	M-392-103	3-29-00
SUPERSTRUCTURE.....	M-392-104	3-29-00
SUPERSTRUCTURE DETAILS.....	M-392-105	3-29-00
SUPERSTRUCTURE DETAILS.....	M-392-106	3-29-00
SUPERSTRUCTURE DETAILS.....	M-392-107	3-29-00
PRECAST T-BEAM DETAIL'S		
T-BEAM NO. 1 & 5.....	M-392-109	3-29-00
T-BEAM NO. 2 & 6.....	M-392-110	3-29-00
T-BEAM NO. 3 & 7.....	M-392-111	3-29-00
T-BEAM NO. 4 & 8.....	M-392-112	3-29-00
T-BEAM DETAILS.....	M-392-113	3-29-00
ABUTMENT NO. 1.....	M-392-114	3-29-00
ABUTMENT NO. 1 DETAILS.....	M-392-115	3-29-00
ABUTMENT NO. 2.....	M-392-116	3-29-00
ABUTMENT NO. 2 DETAILS.....	M-392-117	3-29-00
BENT NO. 1, 2 & 3.....	M-392-118	3-29-00
BENT NO. 1, 2 & 3 DETAILS.....	M-392-119	3-29-00
FINAL FOUNDATION DATA.....	M-392-120	
BILL OF STEEL.....	M-392-121	3-29-00

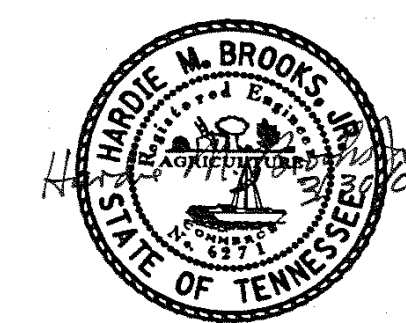
LIST OF REFERENCE DRAWINGS

1958 ORIGINAL BRIDGE	G-13-115 THRU G-13-121
1989 WIDENING	M-211-1 THRU M-211-15

2019 ADT = 164,380
2 @ 76'-4" ROADWAY w/STD-1-1 BRIDGERAIL
DESIGN SPEED = 70 MPH.
STATE OF TENNESSEE

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

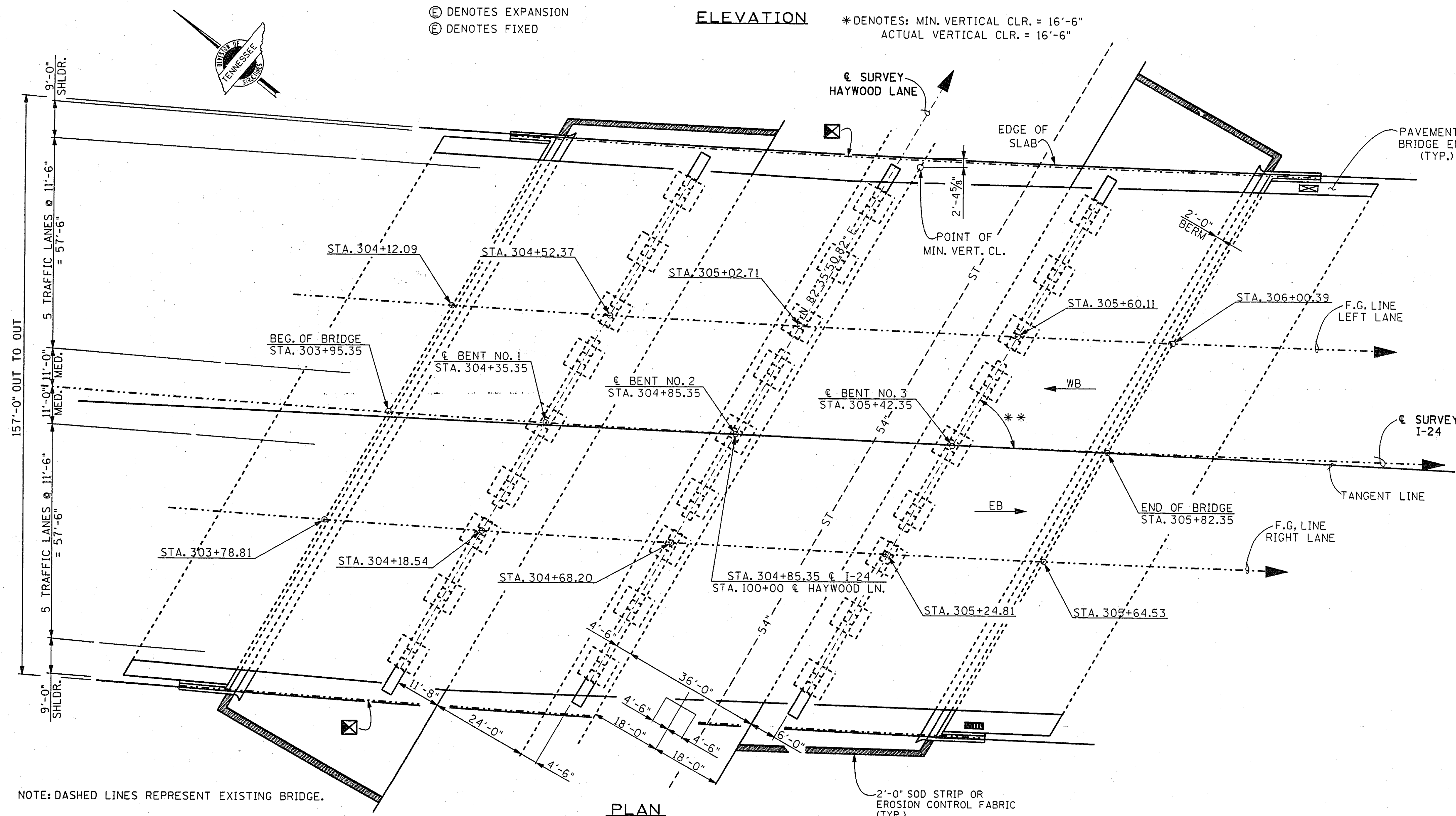
BRIDGE NO. 6
LAYOUT
WIDENING I-24
OVER
HAYWOOD LANE
STATION 304+85.35
LOG MILE 18.47
DAVIDSON COUNTY
2000



CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

M-392-102

TEMPORARY MEDIAN BARRIER = 472 L.F.



NOTE: DASHED LINES REPRESENT EXISTING BRIDGE.

PLAN

---ST--- DENOTES 54" STORM SEWER.

NOTES: DENOTES: END OF BRIDGE DRAIN (2' X 8'-7") REQUIRED.
SEE STANDARD DWG. NO. STD-1-6, 7 & 8.

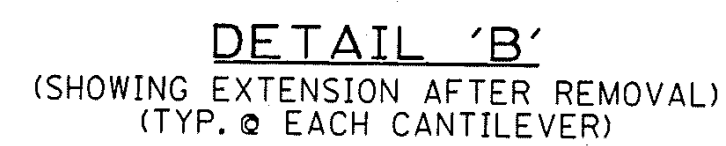
☒ DENOTES: END OF BRIDGE DRAIN (4' X 8'-7") REQUIRED.
SEE STANDARD DWG. NO. STD-1-6, 7 & 9.

☒ DENOTES 3" CONDUIT TO BE INSTALLED W/2 PULL BOXES. ①

** DENOTES: ALL SUBSTRUCTURES SKEWED 60° TO
 LOCAL TANGENT @ STA. 304+85.35.

DESIGNED BY T. JOHNSON DATE 08-99
 DRAWN BY DEBRA LAMAY DATE 12-99
 SUPERVISED BY RLH/HMB DATE 08-99
 CHECKED BY MIN XU DATE 12-99

11:07



* DENOTES: SEE GROUTED BARS IN DRILLED HOLES
NOTE DWG. NO. M- - .



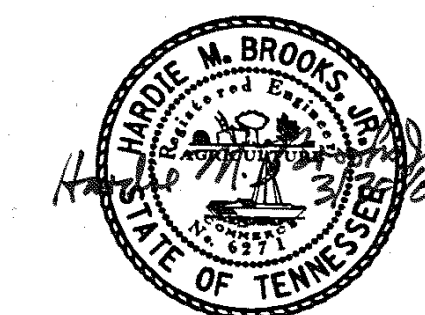
- 1) NO CONSTRUCTION JOINT MAY BE LOCATED CLOSER THAN 10 FT OR FURTHER THAN 15 FT. FROM AN INTERIOR SUPPORT.
- 2) THE SLAB IN THE MIDDLE SECTION OF BOTH ADJACENT SPANS MUST BE POURED TO WITHIN AT LEAST 15 FT OF THE SUPPORTS EITHER PRIOR TO OR CONCURRENTLY WITH THE SLAB OVER AN INTERIOR SUPPORT.



NOTE: NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.

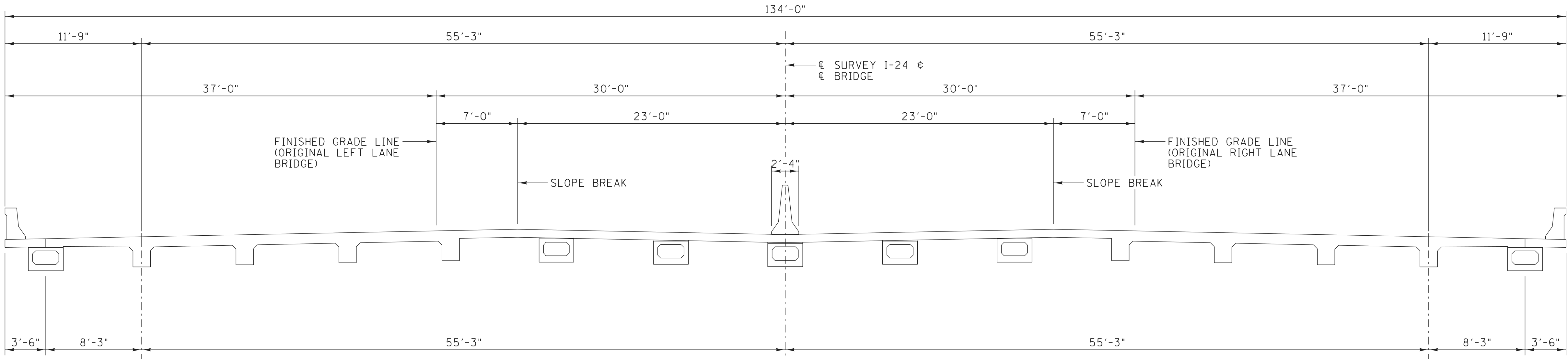
CLASS "D" CONCRETE (BRIDGE DECK) C.Y.	STEEL BAR REINFORCEMENT (BRIDGES) LB.	STEEL BAR REIN. EPOXY (BRIDGES) LB.
118	694	24,180

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
SUPERSTRUCTURE
WIDENING OF
INTERSTATE 24
OVER
HAYWOOD LANE
STATION 304+85.35
DAVIDSON COUNTY
2000



CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

PROJECT NO.		YEAR		SHEET NO.	
19106-4102-04		2021			
REVISIONS					
NO.	DATE	BY	BRIEF DESCRIPTION		
	</				



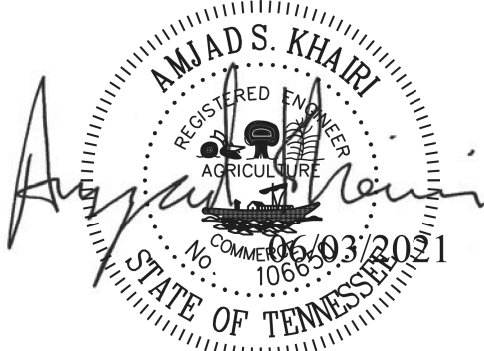
TYPICAL EXISTING CROSS SECTION
SCALE: 3/16" = 1'-0"

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE REPAIR DETAILS

19-10024-23.17 OVER
MILL CREEK
BRIDGE NO. 19100240039

DAVIDSON COUNTY
2021



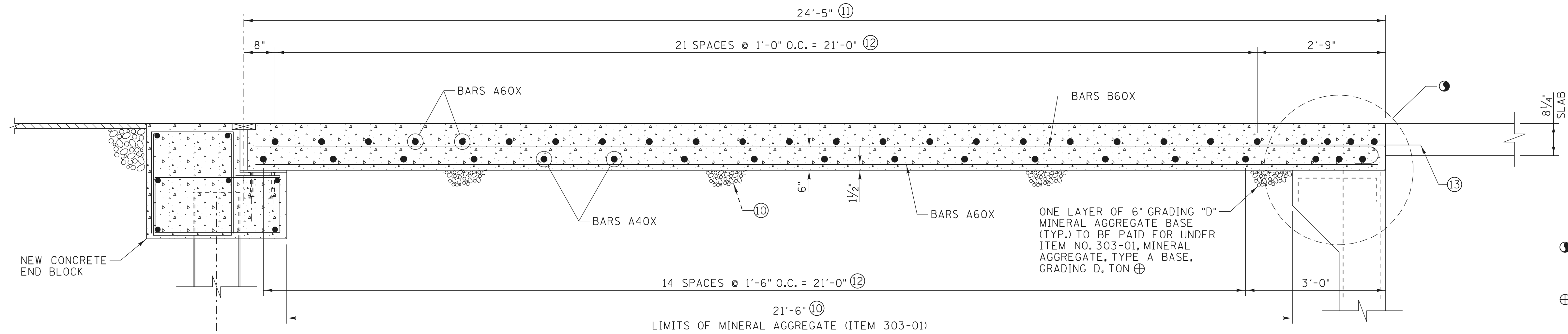
DESIGNED BY J. L. HALBROOK DATE DEC. 2020
DRAWN BY C. W. THOMAS DATE DEC. 2020
SUPERVISED BY J. H. RUDDELL DATE DEC. 2020
CHECKED BY A. J. KHAIRI DATE DEC. 2020

TN D.O.T. ENGINEERING SUPERVISOR M. LAWSON

[illegible]

NOTES:
FOR BAR CONNECTOR DETAIL, SEE DWG. NO. BR-130-902.
FOR DOWEL BAR NOTES AND DETAILS, SEE DWG. NO. BR-130-905.

 DIMENSIONS TAKEN FROM ORIGINAL CONSTRUCTION PLANS. CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS PRIOR TO CONSTRUCTION.

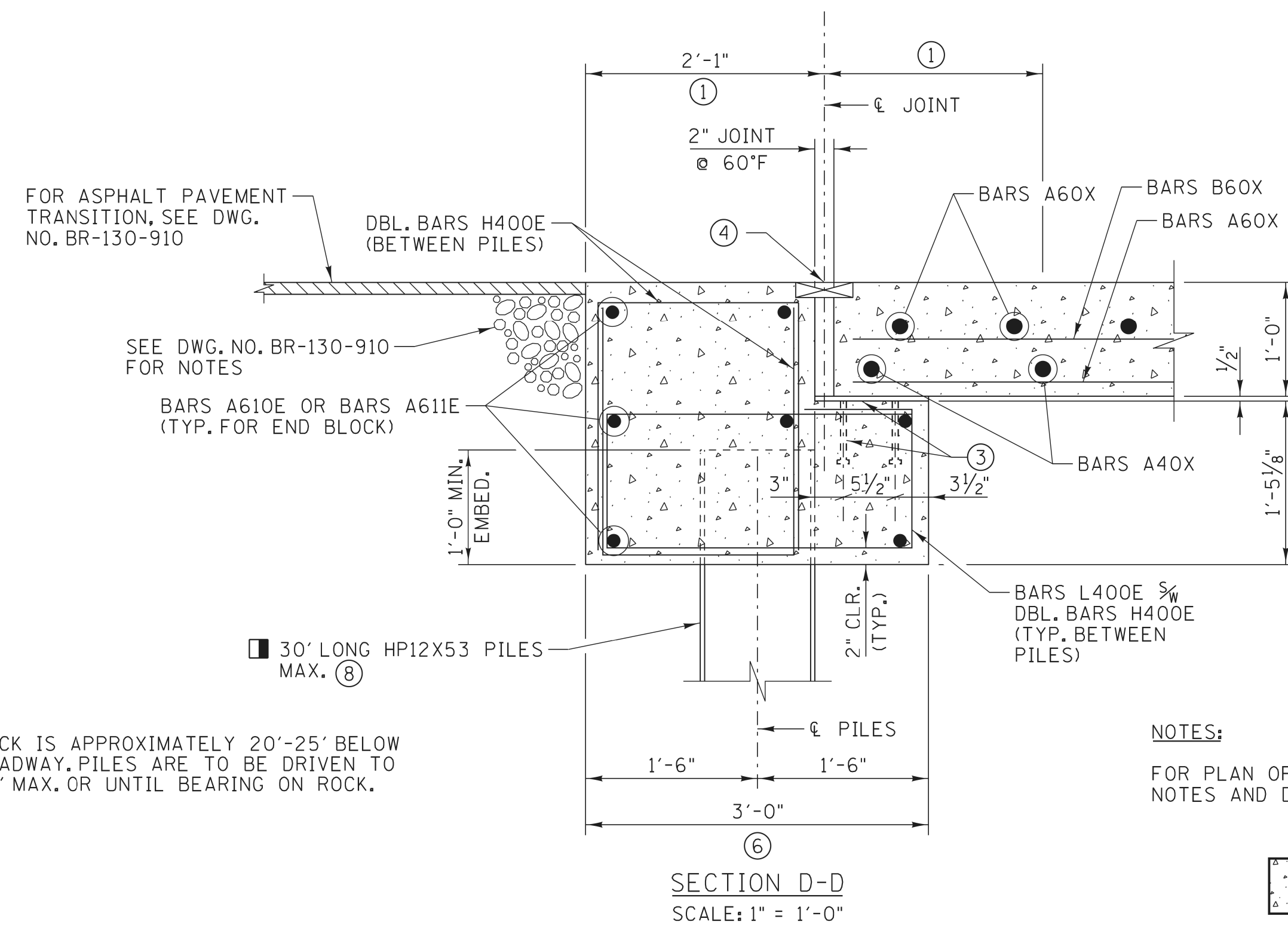


SEE SECTION A-A, SECTION B-B, AND
SECTION C-C ON DWG. NO. BR-130-903.

⊕ ADDITIONAL GRADING "D" MINERAL AGGREGATE MAY BE REQUIRED AT BEGIN AND END OF BRIDGE OF ORIGINAL STRUCTURE PORTION IF NO APPROACH BRACKET IS PRESENT. COSTS ASSOCIATED WITH ADDITIONAL MATERIAL AND LABOR TO BE INCLUDED UNDER 303-01, MINERAL AGGREGATE, TYPE A BASE, GRADING D, TON.

SECTION E-E - APPROACH SLAB CONSTRUCTION ELEVATION

SCALE: $\frac{3}{4}" = 1'-0"$



ROCK IS APPROXIMATELY 20'-25' BELOW ROADWAY. PILES ARE TO BE DRIVEN TO 30' MAX. OR UNTIL BEARING ON ROCK.

NOTES:

FOR PLAN OF END BLOCK AND ADDITIONAL
NOTES AND DETAILS, SEE DWG. NO. BR-130-908

- ① ALL CONCRETE POURS SHALL BE WELL CONSOLIDATED BEHIND AND AROUND THE EXPANSION JOINT STEEL ASSEMBLIES.
- ② EXTREME CARE SHALL BE TAKEN WHEN REMOVING CONCRETE APPROACH SLAB SO AS NOT TO DAMAGE THE EXISTING REINFORCING STEEL TO REMAIN. ALL EXISTING REINFORCING STEEL SHALL BE CLEANED AND INCORPORATED WITH NEW REINFORCING STEEL. ALL WORK MUST MEET WITH FULL SATISFACTION OF THE ENGINEER.
- ③ 1/2" STEEL PLATE (GR 50W) WITH TWO LINES OF 1/2" STUDS SPA. @ 12" FOR FULL WIDTH OF END BLOCK. BEFORE APPROACH SLAB IS CAST, PLATE SHALL RECEIVE A COATING OF AN APPROVED LUBRICANT AND THE CONTRACTOR SHALL PLACE 1 LAYER OF FELT PAPER BETWEEN THE PLATE AND CONCRETE APPROACH SLAB.
- ④ SEE EXPANSION JOINT REPLACEMENT DETAILS, TYPE "H" ON STANDARD DRAWING NO. SBR-2-120. FOR FURTHER DETAILS AND NOTES, SEE STANDARD DRAWING NOS. SBR-2-115, SBR-2-116, AND SPECIAL PROVISION NO. 604S.
- ⑤ CONCRETE FOR ALL EXPANSION JOINT REPAIR AREAS TO BE HIGH EARLY STRENGTH CONCRETE WITH A COMPRESSIVE STRENGTH OF 3500 PSI AT 18 HOURS.
- ⑥ ALL COSTS OF LABOR, MATERIAL, EQUIPMENT AND INCIDENTALS REQUIRED TO INSTALL NEW REINFORCEMENT AND HIGH EARLY STRENGTH CONCRETE FOR NEW END BLOCKS, NEW 1/2" STEEL PLATES AND EMBEDDED STUDS, APPROVED LUBRICANT, AND NEW TYPE "H" STRIP SEAL JOINTS AT THE ENDS OF BOTH APPROACH SLABS AT BRIDGE NO. 19-10024-23.17 TO BE INCLUDED UNDER ITEM NO. 604-10.47, EXPANSION JOINT REPAIRS (TYPE "H"), L.F.
- ⑦ COST OF INCIDENTALS SUCH AS WELDING, CLEANING, SAW CUTTING, DRILLING, ETC. AND MISCELLANEOUS MATERIALS NECESSARY TO COMPLETE THE INSTALLATION OF THE NEW EXPANSION JOINTS AT BOTH APPROACH SLABS SHALL BE INCLUDED UNDER ITEMS BID ON.
- ⑧ ALL COSTS ASSOCIATED WITH DRIVING HP12X53 PILES AT CONCRETE END BLOCKS SHALL BE INCLUDED UNDER ITEM NON. 606-03.03, STEEL PILES (12 INCH), L.F. THIS ITEM ALSO COVERS COSTS ASSOCIATED WITH TRANSPORTING THE PILES.
- ⑨ SET ALL EXPANSION JOINT REPLACEMENTS 2" (TWO INCHES) AT MID-TEMPERATURE OF 60°F., WITH A TOTAL MOVEMENT REQUIRED OF 2" (TWO INCHES), ACTUAL SETTING AS PER TEMPERATURE CHART ON EXPANSION JOINT SHOP DRAWINGS.
- ⑩ PRIOR TO PLACEMENT OF NEW CONCRETE APPROACH SLABS, THE CONTRACTOR SHALL PLACE 6" MINERAL AGGREGATE BASE CONFORMING TO CLASS 'A' GRADING D UNDER THE NEW APPROACH SLAB OF BRIDGE NO. 19-10024-23.17. CONTRACTOR SHALL ENSURE THAT BASE UNDER THE APPROACHES IS PROPERLY SET PRIOR TO FORMING AND PLACING NEW CONCRETE APPROACH SLABS. COST OF 6" GRADING 'D' AGGREGATE BASE SHALL BE INCLUDED UNDER ITEM NO. 303-01, MINERAL AGGREGATE, TYPE A BASE, GRADING D, TON.
- ⑪ ALL COSTS OF LABOR, MATERIAL AND INCIDENTALS TO FORM AND POUR NEW HIGH EARLY STRENGTH CONCRETE FOR NEW APPROACH SLABS AND TOP OF BACKWALL AS REQUIRED SHALL BE INCLUDED UNDER ITEM NO. 604-10.12, CONCRETE SLAB REPAIRS, C.Y.
- ⑫ COST OF ALL REINFORCING STEEL FOR THE NEW APPROACH SLABS AND PORTIONS NOF MEDIAN BARRIER SHALL BE PAID FOR UNDER ITEM NO. 604-10.18, REINFORCING STEEL (REPAIRS), LB.
- ⑬ BARS SHOWN TO BE EMBEDDED INTO ABUTMENT BACKWALL SHALL BE SECURED USING HILTI RE500 EPOXY ANCHOR OR APPROVED EQUAL. ALL BARS SHALL BE EMBEDDED TO DEPTH SHOWN ON DETAILS THIS SHEET AND EPOXY ANCHOR SHALL BE INSTALLED IN STRICT ACCORDANCE WITH MANUFACTURER'S INSTRUCTIONS. HOLES FOR EMBEDDED BARS SHALL BE DRILLED 1/4" LARGER IN DIAMETER THAN THE BAR.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE REPAIR DETAILS

19-I0024-23.17 OVER
MILL CREEK
BRIDGE NO. 19I00240039

DAVIDSON COUNTY
2021

BR-130-904

WASPradlin 6/3/2021 9:25:16 AM
WORKSPACE: TDOT Bridge (2020)
1:208180817434 - TDOT Bridge Repair 124 Davidson CountyDrawings\BRC\03 - Certified\02 - 124146-01 - Structures - BR-130-304.dgn

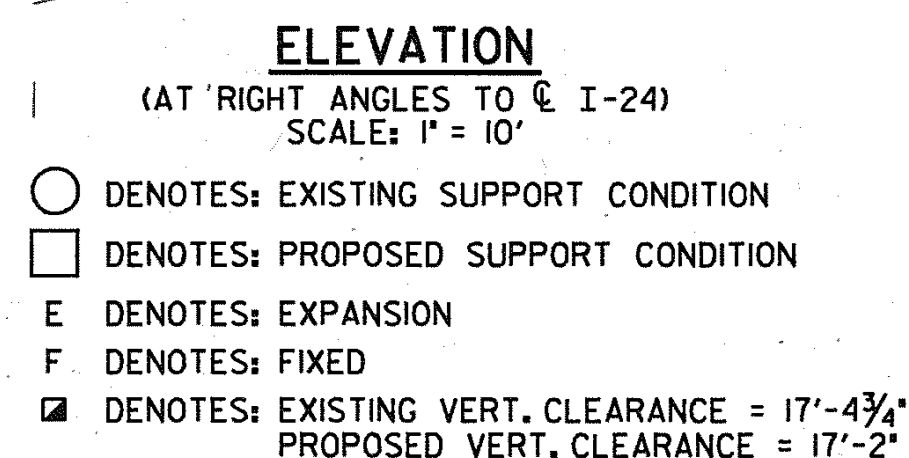


DESIGNED BY M. A. SPRADLIN DATE DEC. 2020
 DRAWN BY C. W. THOMAS DATE DEC. 2020
 SUPERVISED BY J. H. RUDDELL DATE DEC. 2020
 CHECKED BY A. J. KHAIRI DATE DEC. 2020

TN D.O.T. ENGINEERING SUPERVISOR M. LAWSON

[illegible]

NOTE: EXISTING GROUT-FACED SLOPE IN MEDIAN AREA TO BE REMOVED AS NECESSARY FOR INSTALLATION OF ABUTMENTS. AFTER ABUTMENT CONSTRUCTION IS COMPLETE, AREAS OF GROUT WHICH HAVE BEEN REMOVED TO FACILITATE ABUTMENT CONSTRUCTION SHALL BE REPLACED TO ORIGINAL LIMITS WITH A 3" MINIMUM THICKNESS OF CLASS "A" CONCRETE. THE COST OF PARTIAL REMOVAL OF EXISTING GROUT AND REPLACEMENT WITH CLASS "A" CONCRETE SHALL BE INCLUDED IN OTHER ITEMS BID.



<u>DWG. NO.</u>	<u>LAST REV. DATE</u>	<u>DRAWING</u>
* STD-1-3	4-28-97	STANDARD CONCRETE MEDIAN BARRIER
* STD-1-5	4-28-97	REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS
* STD-4-1	4-28-97	STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS
STD-4-2	6-10-96	STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS DESIGN CRITERIA
STD-4-3	6-10-96	STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS
STD-4-4	6-10-96	STANDARD PRECAST PRESTRESSED BRIDGE DECK PANELS CONSTRUCTION DETAILS
STD-5-1	10-25-93	STANDARD PILE DETAILS
STD-5-2	10-26-92	STANDARD PILE DETAILS
STD-6-1	5-01-95	STANDARD SEISMIC DETAILS
STD-9-1	12-19-94	REINFORCING BAR SUPPORT DETAILS FOR CONCRETE SLABS
STD-10-1	5-11-92	MISCELLANEOUS ABUTMENT AND DRAINAGE DETAILS
STD-14-3		STANDARD DETAILS FOR PRESTRESSED BOX BEAMS
SBR-2-124	1-04-96	DETAILS SHOWING REPLACEMENT OF EXISTING BRIDGERAIL SYSTEM WITH NEW JERSEY SHAPE CONCRETE PARAPET AND NEW 10'-2" ENDPST
SBR-2-125	1-04-96	DETAILS SHOWING REPLACEMENT OF EXISTING BRIDGERAIL SYSTEM WITH NEW JERSEY SHAPE CONCRETE PARAPET AND NEW 10'-2" ENDPST
SBR-2-126	1-04-96	DETAILS SHOWING REPLACEMENT OF EXISTING BRIDGERAIL SYSTEM WITH NEW JERSEY SHAPE CONCRETE PARAPET AND NEW 10'-2" ENDPST

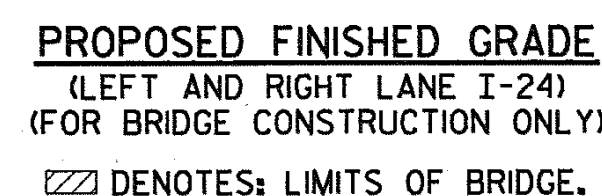
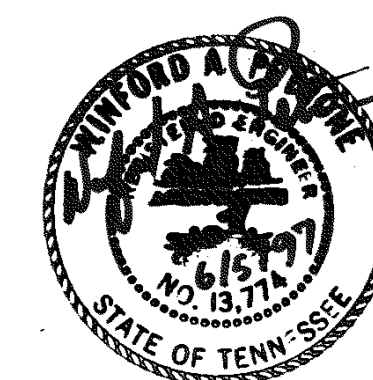
<u>NO.</u>	<u>LAST REV. DATE</u>	<u>REGARDING</u>
105A	3-06-95	APPROVAL OF SHOP DRAWINGS

DWG. NO.	LAST REV. DATE	DRAWING
M-353-53		ELEVATION OF BRIDGE NO. 1
M-353-54		PLAN OF BRIDGE NO. 1
M-353-55		GENERAL NOTES
M-353-56		ESTIMATED QUANTITIES
M-353-57		FOUNDATION DATA
M-353-58		SUPERSTRUCTURE
M-353-59		PHASED CONSTRUCTION DETAILS
M-353-60		SUPERSTRUCTURE DETAILS
M-353-61		SUPERSTRUCTURE DETAILS
M-353-62		SUPERSTRUCTURE DETAILS
M-353-63		SUPERSTRUCTURE DETAILS
M-353-64		BRIDGE SCREED
M-353-65		PRESTRESSED BEAM DETAILS
M-353-66		PRESTRESSED BEAM DETAILS
M-353-67		ABUTMENT NO. 1
M-353-68		ABUTMENT NO. 2
M-353-69		BENT NO. 1 AND NO. 2
M-353-70		FINAL FOUNDATION DATA
M-353-71		BILL OF STEEL

DWG. NOS. G-13-133 THRU G-13-138, G-13-118
AND M-200-127 THRU M-200-129

127'-2" ROADWAY WITH MEDIAN BARRIER RAIL
AND TYPE SBR-2-125 CONCRETE ENDPOST

ELEVATION OF BRIDGE NO. 1
LEFT AND RIGHT LANES
WIDENING OF INTERSTATE 24
OVER
BELL ROAD
STATION 429+89.39 (LOG MI. 20.86)
BR. ID. NO. 19100240041
DAVIDSON COUNTY
1997



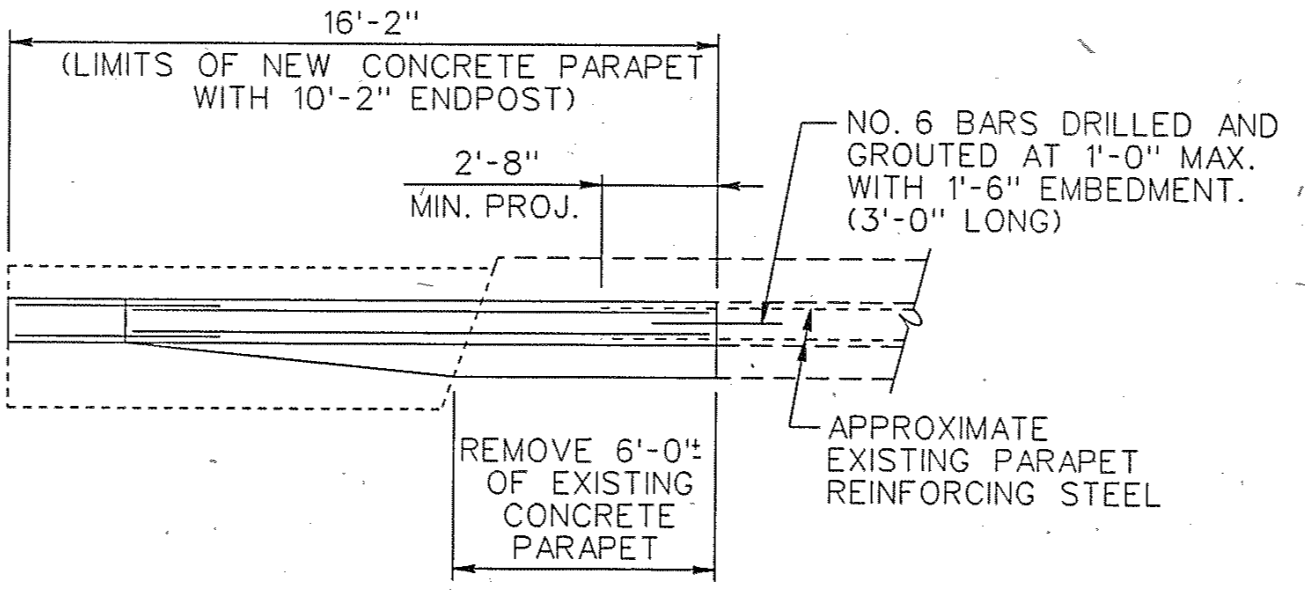
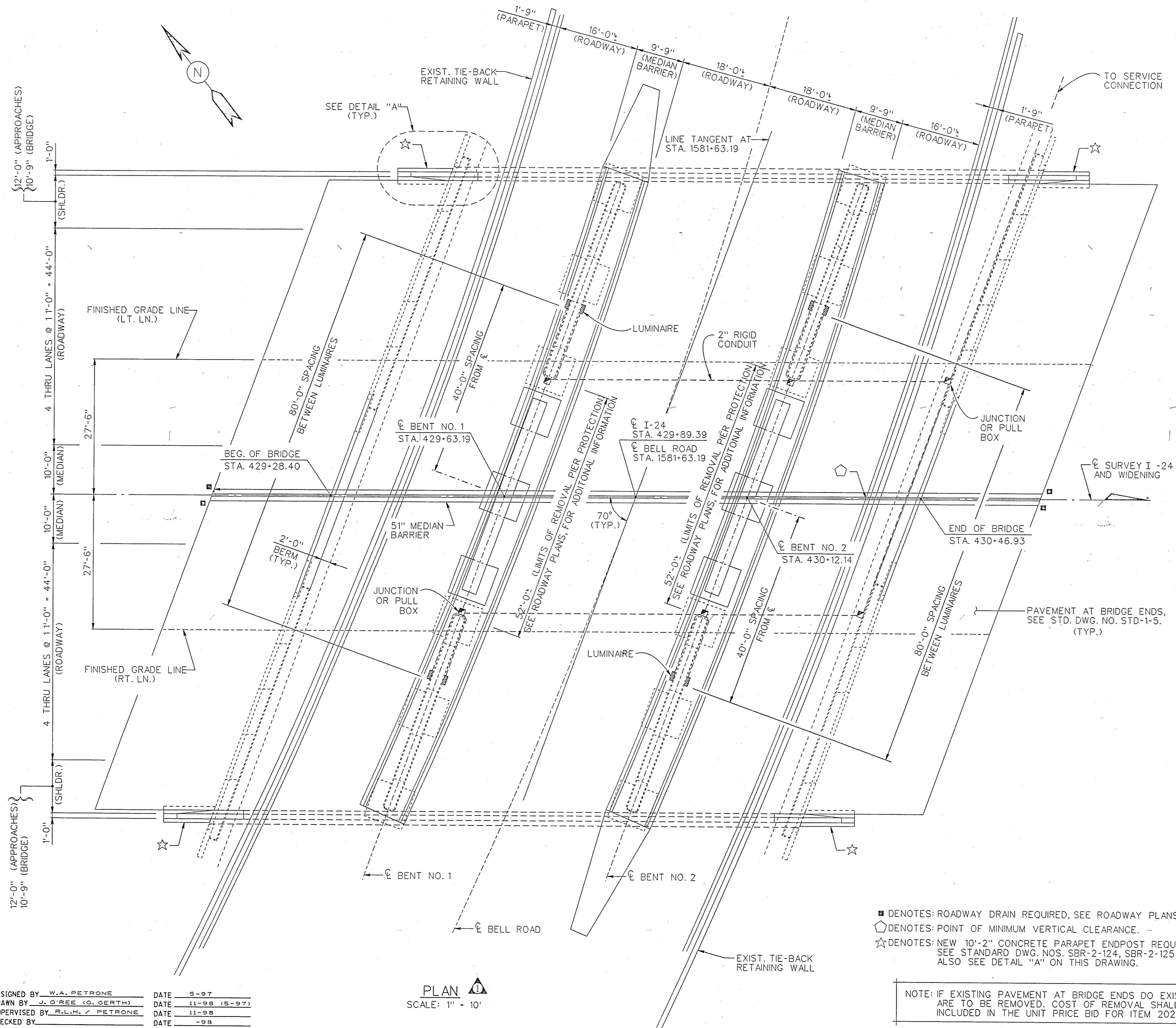
DESIGNED BY W. A. PETRONE DATE 5/97
 DRAWN BY G. GERTH DATE 5/97
 SUPERVISED BY PETRONE DATE 5/97
 CHECKED BY WILSON, PETRONE DATE 5/97

M-353-53

BWSC\$USER\$
28-MAY-1997 1319
c:\dgn\11496-91\49691lay.dgn

PROJECT NO.	YEAR	SHEET NO.
IM-24-1(7)59	1997	

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	11/23/98	HMB	LIGHTING



NOTE: PROVISIONS SHALL BE MADE DURING PARAPET REMOVAL TO PROTECT THE EXISTING LONGITUDINAL REINFORCEMENT. THE EXISTING REINFORCING STEEL SHALL BE COMPLETELY CLEANED, REALIGNED, AND INCORPORATED INTO THE NEW PARAPET AND ENDPOST CONSTRUCTION.

NOTE: THE COST OF REMOVING AND DISPOSING OF THE EXISTING CONCRETE BRIDGERAIL WITHIN THE LIMIT SHOWN IS TO BE INCLUDED IN ITEM NO. 202-04.01, REMOVAL OF STRUCTURES, L.S.

DESIGNED BY W.A. PETRONE DATE 5-97
 DRAWN BY J.O'REE (O. GERTH) DATE 11-98 (5-97)
 SUPERVISED BY R.L.H. / PETRONE DATE 11-98
 CHECKED BY DATE -99

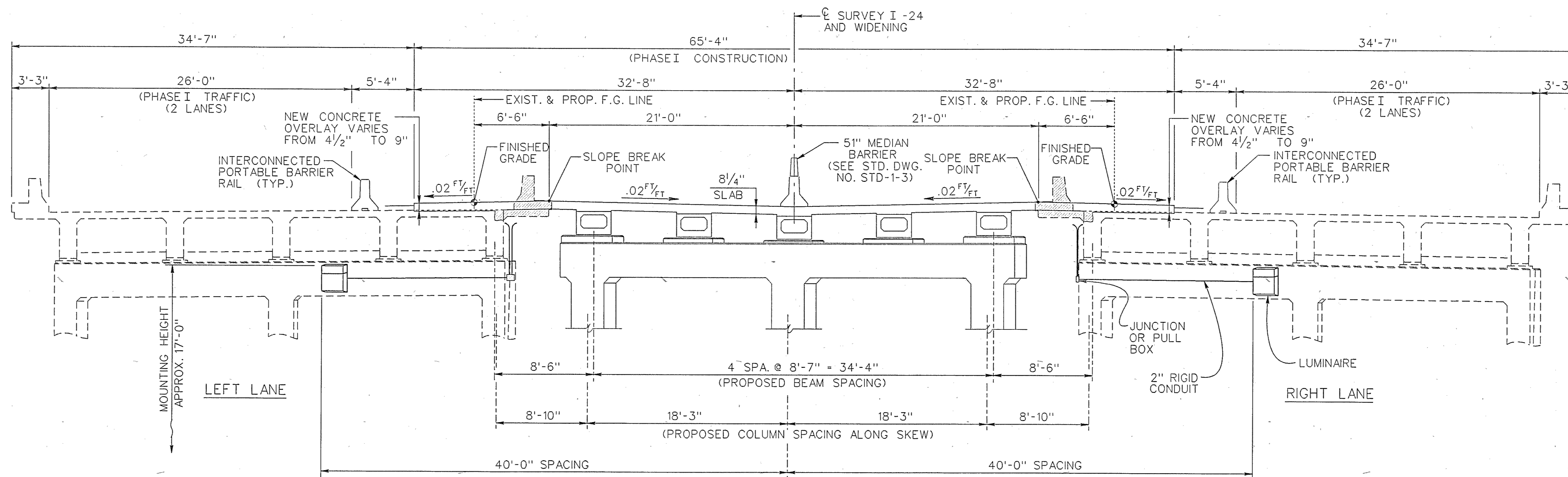
PLAN
 SCALE: 1" = 10'

■ DENOTES: ROADWAY DRAIN REQUIRED, SEE ROADWAY PLANS FOR LOCATION.
 ○ DENOTES: POINT OF MINIMUM VERTICAL CLEARANCE.
 ☆ DENOTES: NEW 10'-2" CONCRETE PARAPET ENDPOST REQUIRED AT BRIDGE ENDS. SEE STANDARD DWG. NOS. SBR-2-124, SBR-2-125 AND SBR-2-126. ALSO SEE DETAIL "A" ON THIS DRAWING.

NOTE: IF EXISTING PAVEMENT AT BRIDGE ENDS DO EXIST, THEY ARE TO BE REMOVED. COST OF REMOVAL SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 202-04.01.

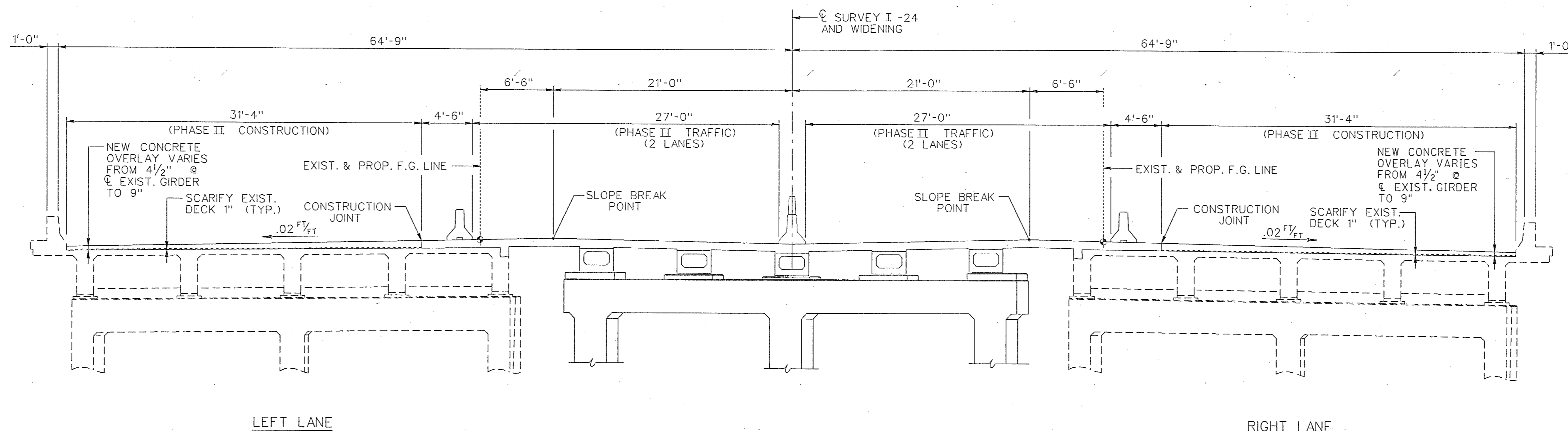
STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 PLAN OF BRIDGE NO. 1
 LEFT AND RIGHT LANES
 WIDENING OF INTERSTATE 24
 OVER
 BELL ROAD
 STATION 429+89.39 (LOG MI. 20.86)
 BR. ID. NO. 19100240041
 DAVIDSON COUNTY
 1997

PROJECT NO.		YEAR	SHEET NO.
IM-24-1(71)59		1997	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	11/23/98	HMB	LIGHTING



TYPICAL CROSS SECTION (PHASE I CONSTRUCTION)
(LOOKING FORWARD ON SURVEY)

■ DENOTES PORTION OF EXISTING STRUCTURE TO BE REMOVED FOR ENTIRE LENGTH OF BRIDGE. (TYPICAL)



TYPICAL CROSS SECTION (PHASE II CONSTRUCTION)
(LOOKING FORWARD ON SURVEY)

PROPOSED CONSTRUCTION SEQUENCE

PHASE I

- 1) SHIFT TRAFFIC TO OUTSIDE LANES OF EACH BRIDGE AS SHOWN AND PLACE TEMPORARY BARRIER RAILS.
- 2) DEMOLISH SHADED AREAS OF DECK AND BRIDGERAIL.
- 3) SCARIFY EXISTING DECK 1" AND COMPLETE PARTIAL OR FULL DEPTH DECK REPAIR AS REQUIRED.
- 4) POUR CENTER CONCRETE DECK AND MEDIAN BARRIER TO LIMITS SHOWN.

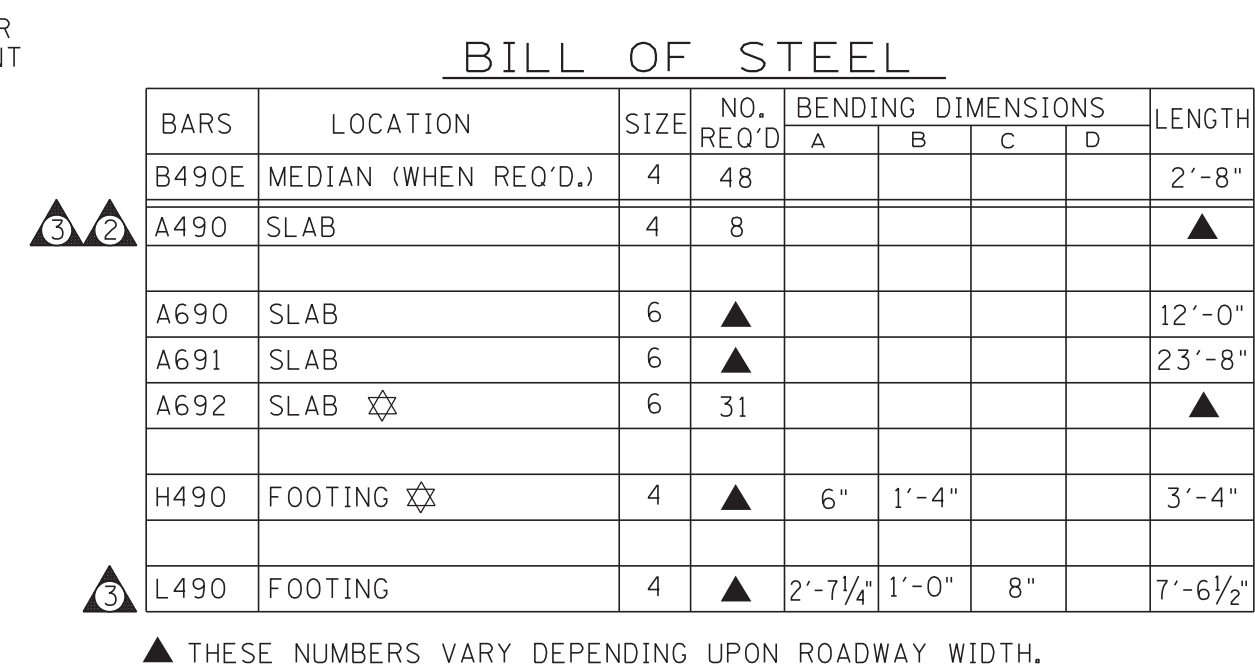
PHASE II

- 1) SHIFT TRAFFIC TO MEDIAN AREA AS SHOWN AND SHIFT TEMPORARY BARRIER RAILS.
- 2) SCARIFY EXISTING DECK 1" AND COMPLETE PARTIAL OR FULL DEPTH DECK REPAIR AS REQUIRED.
- 3) POUR PHASE II CONCRETE DECK.

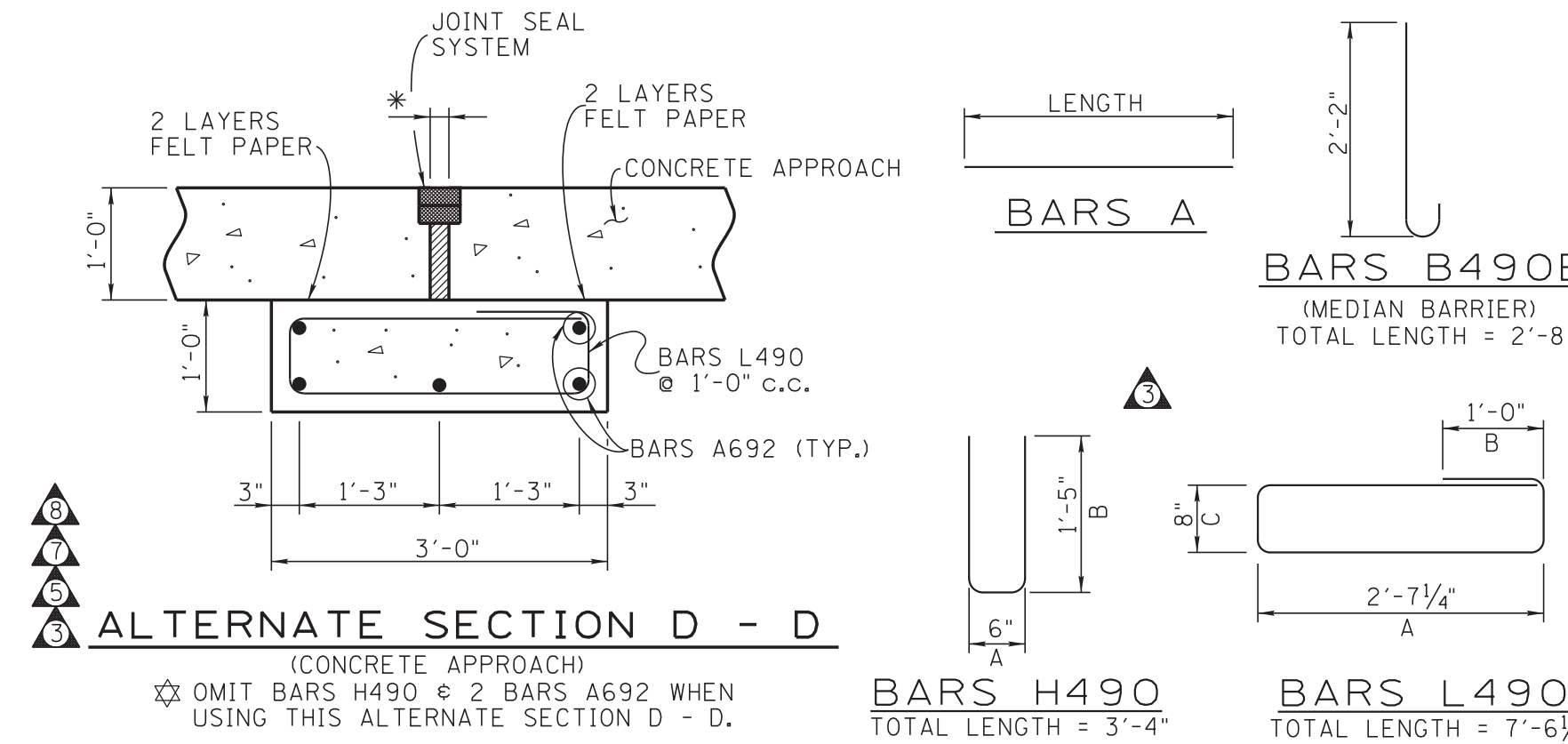
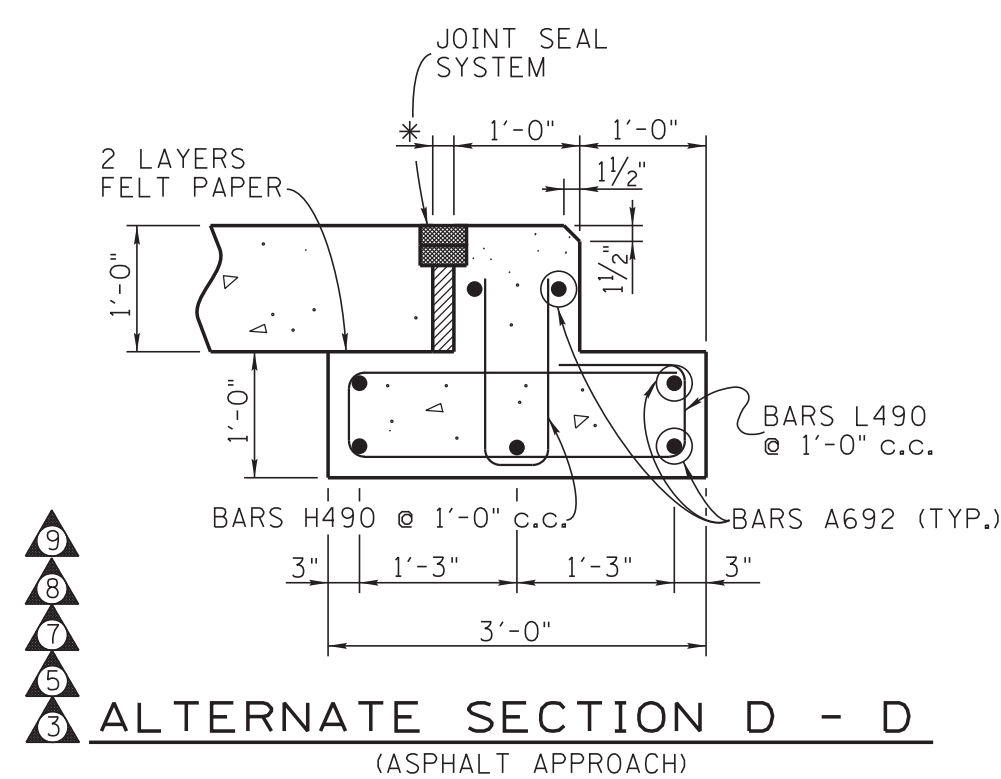
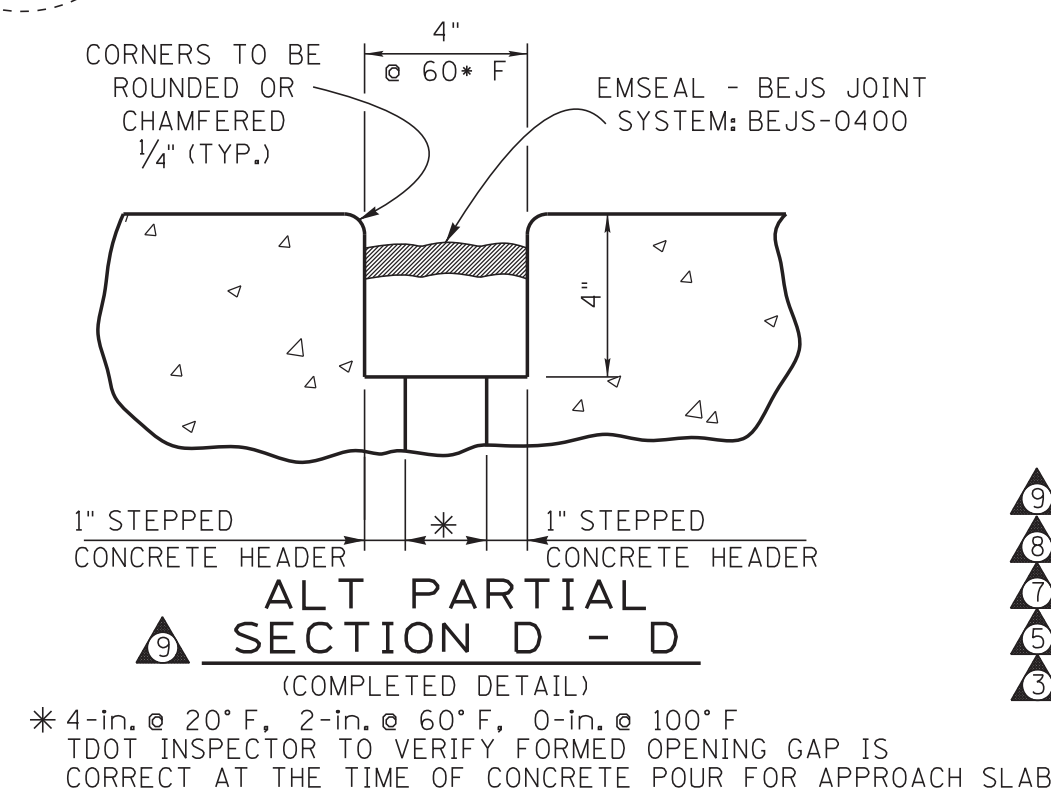
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PHASE CONSTRUCTION DETAILS
LEFT AND RIGHT LANES
WIDENING OF INTERSTATE 24
OVER
BELL ROAD
STATION 429+89.39 (LOG MI. 20.86)
BR. ID. NO. 19I00240041
DAVIDSON COUNTY
1997

DESIGNED BY: W.A. PETRONE DATE: 5-97
DRAWN BY: J.O'REE (G. GERTH) DATE: 11-98 (5-97)
SUPERVISED BY: R.L.H. / PETRONE DATE: 11-98
CHECKED BY: DATE: -98



PROJECT NO.		YEAR	SHEET NO.
		1995	
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	5-1-95	CMH	GENERAL REVISION REDESIGN
2	12-18-95	CMH	ADDED BARS A490 AND JOINT DETAILS
3	4-28-97	CMH	REVISED JOINT DETAILS AND NOTE
4	9-6-99	CMH	REVISED JOINT NOTE
5	7-31-00	CMH	REVISED JOINT DETAILS, NOTE, LIMIT OF PIPE AND ADDED NOTE #3
6	4-8-05	JHW	ADDED NOTE
7	8-8-08	JHW	REVISED JOINT DETAILS AND NOTE
8	6-1-11	WUS	ADDED CONSTRUCTION DETAIL
9	3-26-14	WUS	REVISED JOINT DETAILS AND NOTE
			REVISED JOINT DETAILS AND NOTES.



- ## NOTES
1. QUANTITIES FOR CLASS 'A' CONCRETE, REGULAR AND EPOXY COATED REINFORCING STEEL (WHEN REQUIRED FOR MEDIAN BARRIER), STYROFOAM, GRATE AND MISCELLANEOUS MATERIALS FOR BRIDGE END DRAIN, WHEN REQUIRED, ARE TO BE INCLUDED IN PAVEMENT AT BRIDGE ENDS, S.T. FOR BAR BENDING DIMENSIONS SEE THIS SHEET AND BILL OF STEEL FOR BRIDGE END DRAIN ON DRAWING NO. STD-1-6.
 2. COST OF MINERAL AGGREGATE CLASS A GRADING D BASE QUANTITY SHALL BE INCLUDED IN COST OF PAVEMENT AT BRIDGE ENDS. CLASS B GRADING C OR D MAY ALSO BE USED.
 3. NOTE: TOP OF SLAB AND TOP OF END BEAM TO CONFORM TO ROADWAY SLOPE AND GRADE.

CONCRETE, TO BE CLASS 'A' ($f'_c = 3,000$ psi)
 REINFORCING STEEL; SHALL BE ASTM A615 GRADE 60 UNLESS NOTED
 OTHERWISE, SEE SECTION 604 AND 907 OF THE STANDARD SPECIFICATION
 SPECIFICATIONS; STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE
 TENNESSEE DEPARTMENT OF TRANSPORTATION (CURRENT EDITION).
 NOTE: THE APPROACH SLAB SHALL NOT BE POURED UNTIL THE ADJACENT
 END SPAN DECK SLAB IS IN PLACE AND ACCEPTED BY THE ENGINEER.
 NOTE: THE APPROACH SLAB CONTROL ELEVATIONS SHALL BE ADJUSTED,
 (IF REQUIRED), TO MATCH THE IN PLACE DECK SLAB IN BOTH
 TRANSVERSE AND LONGITUDINAL DIRECTIONS.

- 2 (A) JOINT SEAL SYSTEM:
3 THE EXPANSION JOINT SYSTEM USED SHALL BE ON THE TDOT APPROVED
4 QUALIFIED LISTS FOR ACCEPTABLE PRODUCTS. THE JOINT SYSTEM SHALL
5 BE INSTALLED UNDER THE DIRECT SUPERVISION OF AN AUTHORIZED
6 TECHNICIAN PROVIDED BY THE EXPANSION JOINT SUPPLIER, FOR EACH
7 JOINT AT EACH BRIDGE AND FOR EACH BRIDGE LOCATION WITHIN THE
8 PROJECT. THE TECHNICIAN MUST APPROVE ALL ASPECTS OF THE
9 GEOMETRY AND PREPARATION, INCLUDING GRINDING AND/OR GROOVING,
10 PRIOR TO ANY JOINT MATERIAL INSTALLATION.

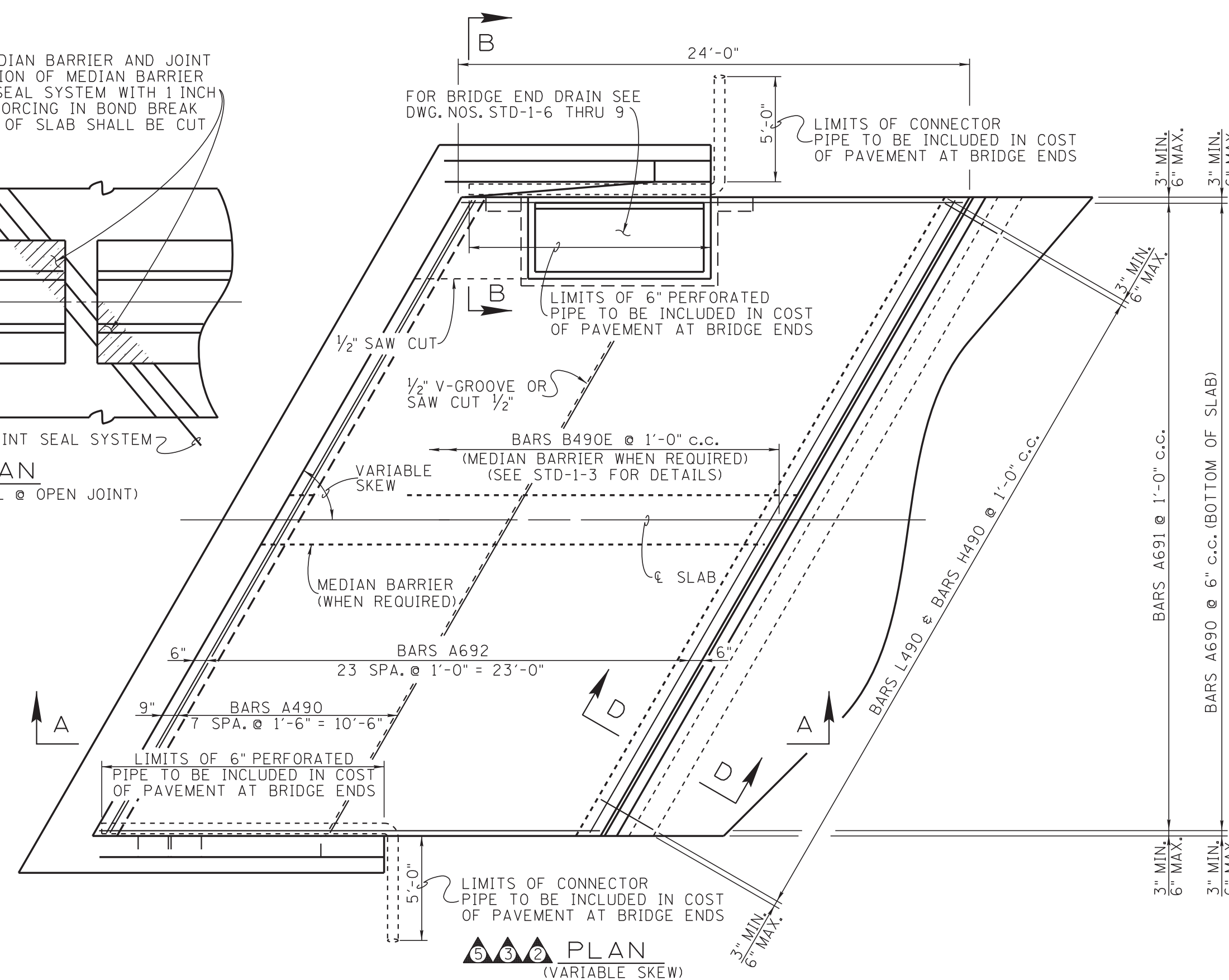
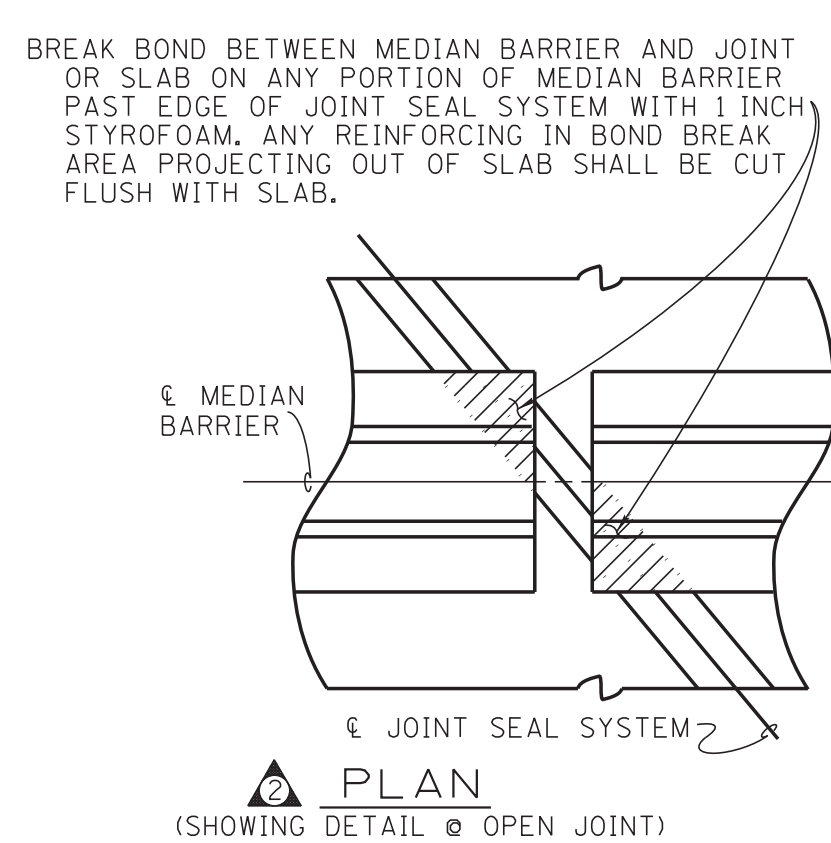
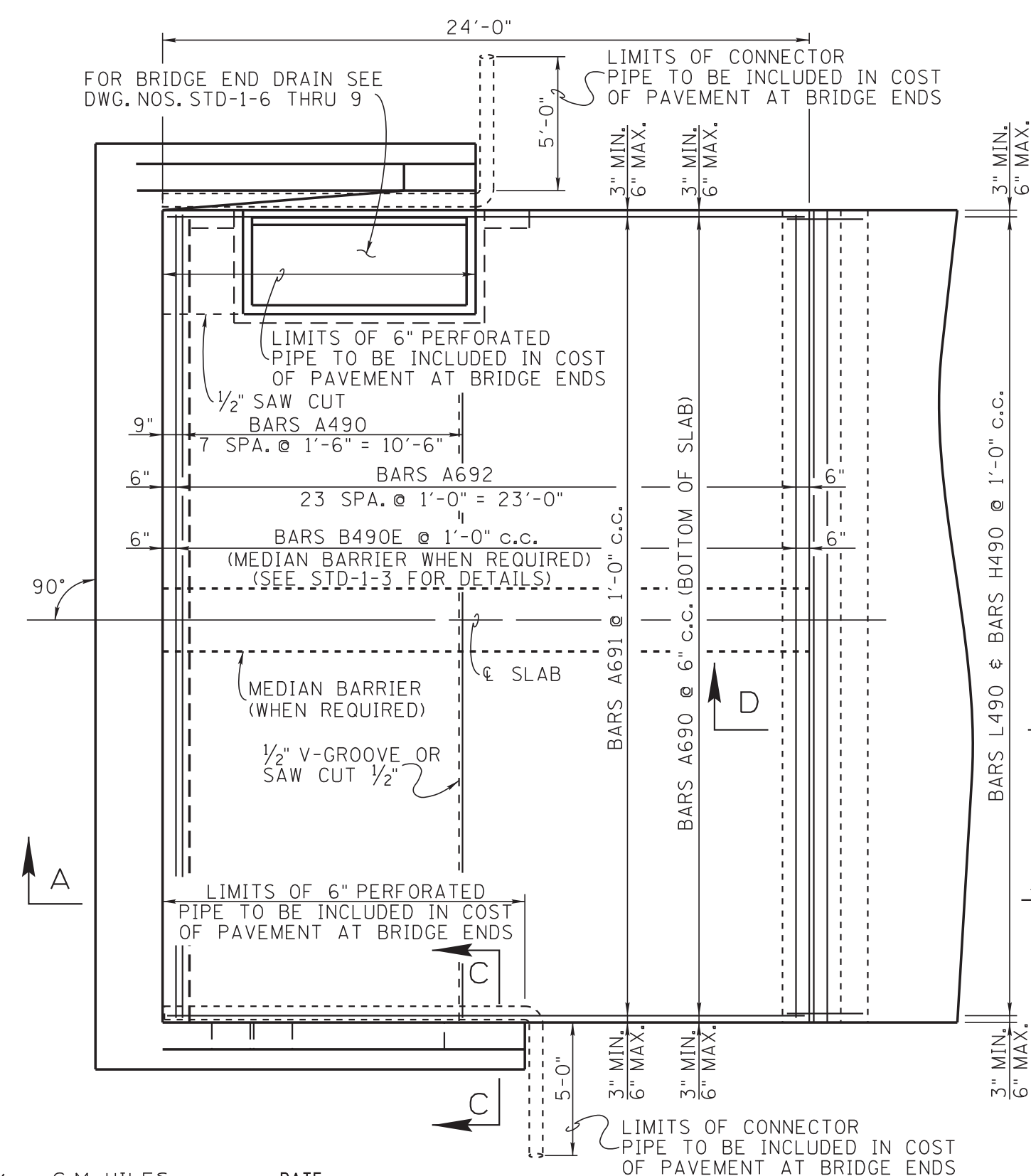
-  NOTE: THE JOINT SEAL SYSTEM IS NOT REQUIRED WHEN THE BRIDGE HAS AN EXPANSION JOINT AT THE ADJACENT ABUTMENT.

M/R MINOR REVISION - FHWA APPROVAL NOT REQUIRED

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
REINFORCED CONCRETE
PAVEMENT AT BRIDGE ENDS
1995

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

SHEET 5 OF
STD-1-5



DESIGNED BY C.M. HILES DATE _____
 DRAWN BY KIM FRANKENFIELD DATE 4-95
 SUPERVISED BY C.M. HILES DATE 4-95
 CHECKED BY _____ DATE _____

PLAN
(90° SKEW)

